

Exhibit B
**NASSAU
CROSSING
PUD**

A William Burgess
District Development

YULEE, FLORIDA

First Amendment 11/26/2018

Matovina & Company

The William Burgess District and by extension the Nassau Crossing PUD is a rail community reinvented through a vibrant and engaging built environment that captures the essence of the railroad’s rich heritage and related cultural progressions and applies that spirit in a new and modern way. The aesthetic is intended to reinvent the social nucleus of Yulee by creating an engaging and creative ‘place’ by incorporating old styles with a new feel. As Lewis Mumford so eloquently stated - *Today we must treat the social nucleus as the essential element in every valid city plan.*



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NASSAU COUNTY 2032 VISION PLAN

Below are specific Goals and Strategies identified in the 2032 Vision Plan which are implementable via the William Burgess District and by extension the Nassau Crossing PUD.

ISSUE AREA 2: MULTI-MODAL TRANSPORTATION

Goal 1: Achieve a network of safe and efficient multi-modal transportation that is capable of meeting the transportation needs of residents and visitors at an acceptable level of service in a safe and efficient manner. Strategy 5 . Designate transit corridors for future public transportation service. These corridors may be designated for greater land use density and intensity.

ISSUE AREA 4: MIXED USE DEVELOPMENT

Goal 1: Encourage mixed-use developments designed to accommodate multiple community activities and services in close proximity. By reducing infrastructure demand, mixed-use developments can generate a positive fiscal impact on County’s financial resources. Strategy 2. Through the Local Planning Agency, establish design guidelines, dimensional criteria, and incentives to promote compact mixed-use development patterns. Characteristics of mixed-use zoning include multiple uses dispersed vertically, shared parking located behind buildings, public amenities such as schools and parks as community focal points, and extensive pedestrian connectivity.

ECONOMIC IMPACTS

As shown in the conclusions from the Fishkind Fiscal Sustainability Study incorporated in this Vision, the types of growth and development, which occur in Nassau County in the future, will have significant impacts on the financial abilities of the County to pay for current and future activities. Some very difficult choices will have to be made in order to ensure Nassau County maintains its current quality of life, pay for future growth, and ensures a continuation of the quality of life so important to residents.

- » Type of Growth Determines Fiscal Impact
- » Not all land uses are created fiscally equal.
- » Certain land uses yield a higher fiscal benefit to Nassau County.
- » At current expenditure levels, typical residential land uses do not pay for themselves unless at very high price points.
- » Office, retail, and industrial land uses have the ability to generate positive fiscal benefit.
- » Mixed-use development also has the ability to generate a positive fiscal benefit for the County.

	Total	NPV
1 Single Family Home	-\$16,530	-\$11,885
50K sf Office	\$888 425	\$225,642
100K sf Retail	\$13,548,801	\$4,830,850
50K sf Industrial	\$471,895	\$48,957
100 Single Family Homes	-\$1,653,043	-\$1,188,506
Mixed Use Development	\$11,819,642	\$3,482,675

Source: Fishkind & Associates, August 2007, Proposal, “Fiscal Sustainability after Tax Reform,” and “Nassau County Fiscal Sustainability Study,” Fishkind & Associates, Inc., September 2, 2008.

NASSAU COUNTY2030 COMPREHENSIVE PLAN

Below are specific Goals and Strategies identified in the 2030 Comprehensive Plan which are implementable via the William Burgess District and by extension the Nassau Crossing PUD.

Policy FL.08.04
The County shall discourage Urban Sprawl by requiring higher density compact development to occur in areas that are planned to be served by public facilities, providing for sound and cost-efficient public facility planning. It will also require lower density development to occur in areas that are environmentally sensitive or in areas that are not planned to receive a high level of public facilities or services.

Policy FL.08.05
The Land Development Code shall provide incentives to direct commercial and multi-family residential uses into clustered or nodal development patterns, that eliminate or reduce strip or ribbon development following major County or state roads.
Policy FL.08.06

Policy FL.08.06
The Land Development Code shall provide incentives to encourage new residential and commercial development in rural and transitioning areas to accomplish the following:

- A. Develop in a pattern that is a logical extension of existing urban development patterns avoiding leapfrog or scattered development.
- B. Develop in clustered or nodal patterns, eliminating or reducing strip-style development along arterial and collector roads.
- C. Develop in a pattern that enhances the potential for the extension and maximization of central (regional) water and sewer systems.
- D. Contribute to the development of mixed-use communities that provide for integrated residential and employment opportunities; and provide for civic and public facilities including emergency medical, fire protection and police facilities. parks and other recreational facilities, schools, hospitals and other public or institutional uses.
- E. Minimize the potential impact of urban development on the agricultural productivity of the areas.
- F. Where appropriate, use enhanced standards to create urban-level infrastructure and design elements for new development, including but not limited to streets, stormwater management facilities, landscaping, and signage.

Policy FL.09.05
The County shall evaluate development plans to ensure that open space is provided for recreation in all proposed residential or mixed use development projects in accordance with the established level of service.

Policy FL.10.01
The Land Development Code shall permit the use of innovative land development techniques and allow for appropriate density bonuses to encourage construction of affordable housing units.

Policy FL.10.06
The Land Development Code shall promote the construction of master planned and mixed use developments by providing incentives such as density bonuses, flexible design standards and funding options for required infrastructure improvements.

WILLIAM BURGESS MIXED USE ACTIVITY CENTER OVERLAY

One of the specific goals expressed in the County's Vision 2032 final report is to encourage mixed-use developments designed to accommodate multiple community activities and services in close proximity. By reducing infrastructure demand, mixed-use developments can generate a positive fiscal impact on County's financial resources. It is the County's objective to coordinate community efforts to develop high-value mixed-use developments that achieve this goal.

The William Burgess Mixed Use Activity Center establishes a model activity center that will be designed to create a quality of place that is integral for quality of life, while also serving to alleviate traffic from the State Road 200/ A1A Corridor.

Centered within ½ mile of the intersection of William Burgess Boulevard and the CSX rail line running parallel to U.S. Hwy. 17, development within the William Burgess Mixed Use Activity Center shall promote sustainable, compact mixed use development. It will include residential, commercial, office, and employment-generating uses. It will promote multi-modal transportation including walking, biking, and future transit systems. This includes the identification and reservation of land to support a future commuter rail station.

A) Specific incentives and design guidelines will be adopted into the Land Development Code that will incorporate the following goals and principles for development of this Overlay:

1. A high level of interconnectivity of multiple modes of transportation to redirect traffic and alleviate the burden on SR 200/A1A.
2. Higher density, mixed use development that provides a strong work-life balance and opportunities for affordable housing and economic development.
3. Strong pedestrian and bicycle orientation that also welcomes potential commuter rail along the US Hwy 17 Corridor.
4. High quality public spaces that create opportunities for cultural experiences and outdoor interaction, entertainment, and play for all ages and ability levels.
5. Diversity of land uses, spaces, building types and styles designed to create a sustainable, compact development and community.
6. Serve as a template for the creation of a Mixed Use Future Land Use Category and mixed-use zoning districts that will serve to guide other compact nodes of development.

B) Within the boundaries of this overlay, the High-Density Residential (HDR) and Commercial (COM) Future Land Use Map (FLUM) designations, residential development may be permitted up to a maximum of twenty (20) units per acre, subject to conformance with goals and principles described above and with adopted design guidelines and performance standards in the Land Development Code. It is encouraged this provision issued in conjunction with the Affordable Housing Density Bonus found in Policy FL.01.03.

C) Within the boundaries of this overlay, the High-Density Residential (HDR) and Commercial (COM) Future Land Use Map (FLUM) designations shall have a maximum Floor Area Ratio (FAR) of 2.0. Where residential and non-residential uses are vertically integrated within the same structure, FAR calculation will exclude those portions devoted to residential dwelling units.

D) Impervious Surface Ratio (ISR) requirements for properties within the overlay will be determined on a case-by-case basis. Creation of a master drainage plan is encouraged.

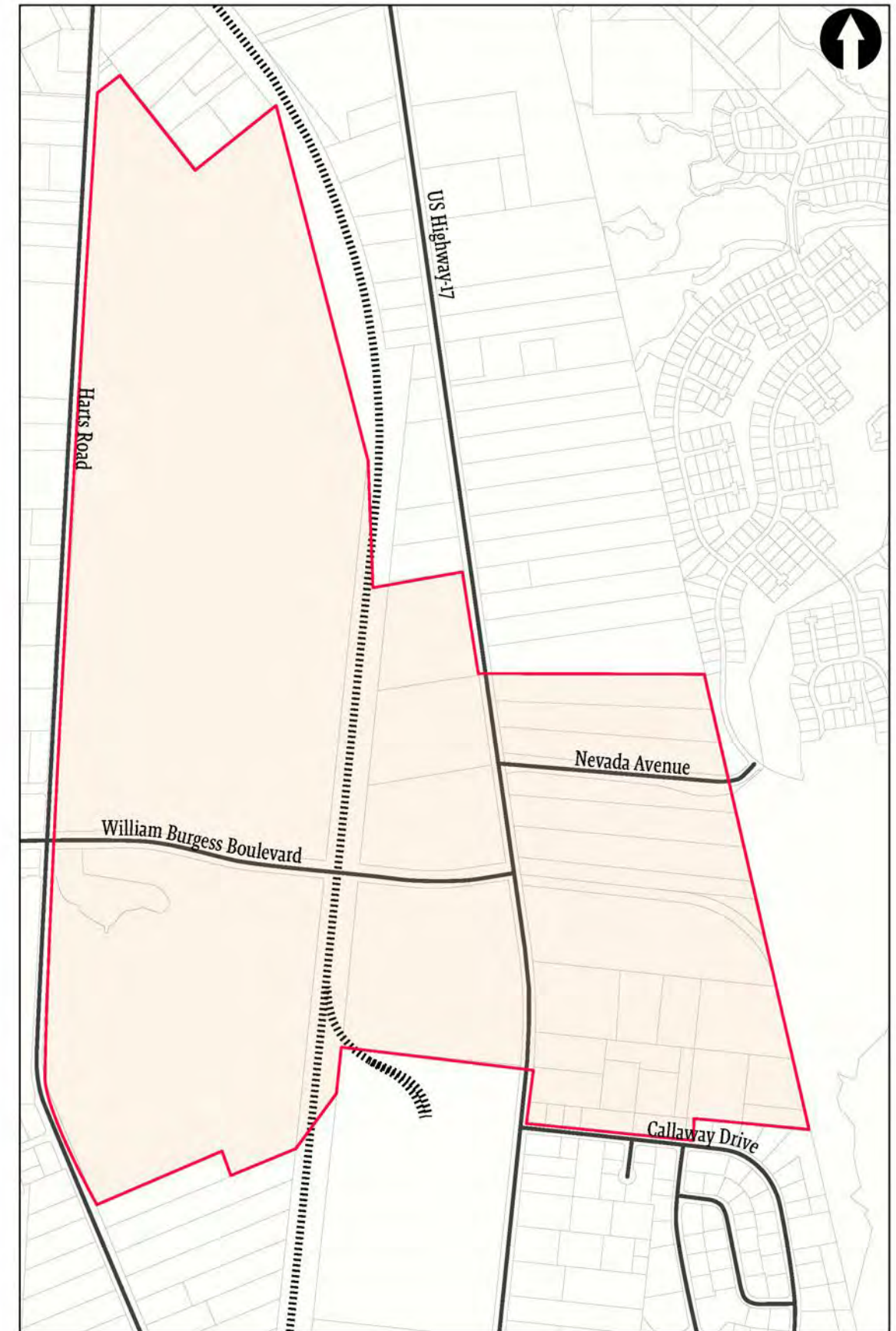


Figure 1. William Burgess Mixed Use Activity Center Overlay Boundary

INTENT

The William Burgess District (WBD) and, by direct extension, the Nassau Crossing PUD represents the philosophical shift in land-use planning away from the unsustainable automobile oriented development pattern that has been prominent in Nassau County over the preceding decades. It is the intent of this PUD to create a sense of place in the form of a compact mixed-use walkable community, designed at a pedestrian scale, that promotes strong social ties and the cultivation of ‘community’ in the inter-personal sense. Through the application of sound land-use planning, urban design and placemaking techniques, each element of the development program is designed to encourage the daily face-to-face interaction of community members which was lost with the sprawling suburban scale development pattern of the last half century.

As such, the WBD and Nassau Crossing PUD were not created within the vacuum of a single tract of land but rather as an integrated component of the greater environment both in a physical and metaphysical manner. It is imperative to recognize that the built environment is not the ‘community’ but rather the vehicle that facilitates the creation of community. As Lewis Mumford so eloquently stated, “...today we must treat the social nucleus as the essential element in every valid city plan...”. Community is cultivated and organically matured through equity, social engagement, shared values and a celebration of diversity. The goal of this project is to facilitate the cultivation of community through the built environment. The goal is not to maximize intensities and densities but rather provide for a healthy mix of uses at the scale necessary to activate the nucleus of the WBD and serve as a catalyst for community formation - to create lasting public value.



THEME

To facilitate a unique, vibrant and diverse community a strong sense of place must be established. To create a sense of place it is not sufficient to merely focus on form and geometric arrangement. There must be a thorough comprehension of the environmental, geographical and historical context in which the project exists. Consistent with the Critical Regionalism movement in architecture, we must reject the sterility of modernism, reject the intent to ignore place and region with pure form and instead seek to use materiality and region as an expression of place. The railroad, particularly in the US, organically captured this philosophy in interesting and profound ways. The railroad was the vein connecting incredible urbanism with ultra-rural towns and villages. While the catalyst for progression was the railroad, the diversity in regionally available materials, climate, terrain and cultural preference resulted in unique and identifiable places.

Like most of the US, the railroad has played a formative role in Nassau County and will continue to shape its future. The Florida Railroad was the catalysts for the creation of compact mixed-use towns in the late 19th and early 20th century. Yulee, then known as the Hart's Road Station, is an example of a town that came to fruition as newly laid rail-lines of the Florida Railroad (1855-1861) intersected with the existing Hart's Road (Isaiah Hart's Jacksonville to St. Marys, GA - circa 1840). While the remnants of the Hart's Road Station and the rail-based community can be seen in 'old Yulee', the automobile oriented development pattern of the second half of the 20th Century has long since overtaken the original development pattern.



Port of Fernandina



The Archer Florida Depot Late 1800's



Callahan, FL circa 1880-1890 -
photo credit: West Nassau Historical Society



Orange City, Florida Depot circa 1860

The WBD and Nassau Crossing PUD are based around the intersection of a rail-line running on a north-south axis radiating out from Jacksonville and a major collector roadway running on an east-west axis providing easy access to SR200/A1A, US Hwy 17 and I-95. As such, given the formative role the railroad has played in this community and will continue to play in the future, it is only fitting the theme/aesthetic of the WBD and Nassau Crossing PUD be Vintage Railroad. But not just Vintage Railroad, following the philosophy expressed in the critical regionalism movement, but rather Vintage Florida Railroad.

The feel of 'vintage Florida railroad' is captured in the rawness of the material, the grandness and power of the application set within a backdrop of a wild and untamed Florida. A colliding of modern man's greatest advancements and Florida's native magnificence. Beauty and raw power are expressed in both. It is the intent of the WBD and the Nassau Crossing PUD to provide balance between beauty and power, raw and finished, elegant and industrious, rustic and urban, and wild and tame. This aesthetic will be captured through the use of powerful materials and lush native landscapes with a mixture of rustic charm and refined modernism. Raw unrefined material infused with cutting-edge technology set within the context of the theme. The aesthetic is not defined by the era (time-based) of the Florida Railroad but rather the materiality and essence of application. The context of the Vintage Florida Railroad provides for limitless combinations of material and application allowing for design freedom that creates a sense of place while not limiting creativity. See the WBD Vision Book. The application of these themes is further defined in herein.



Train on Main St. in Gainesville, FL

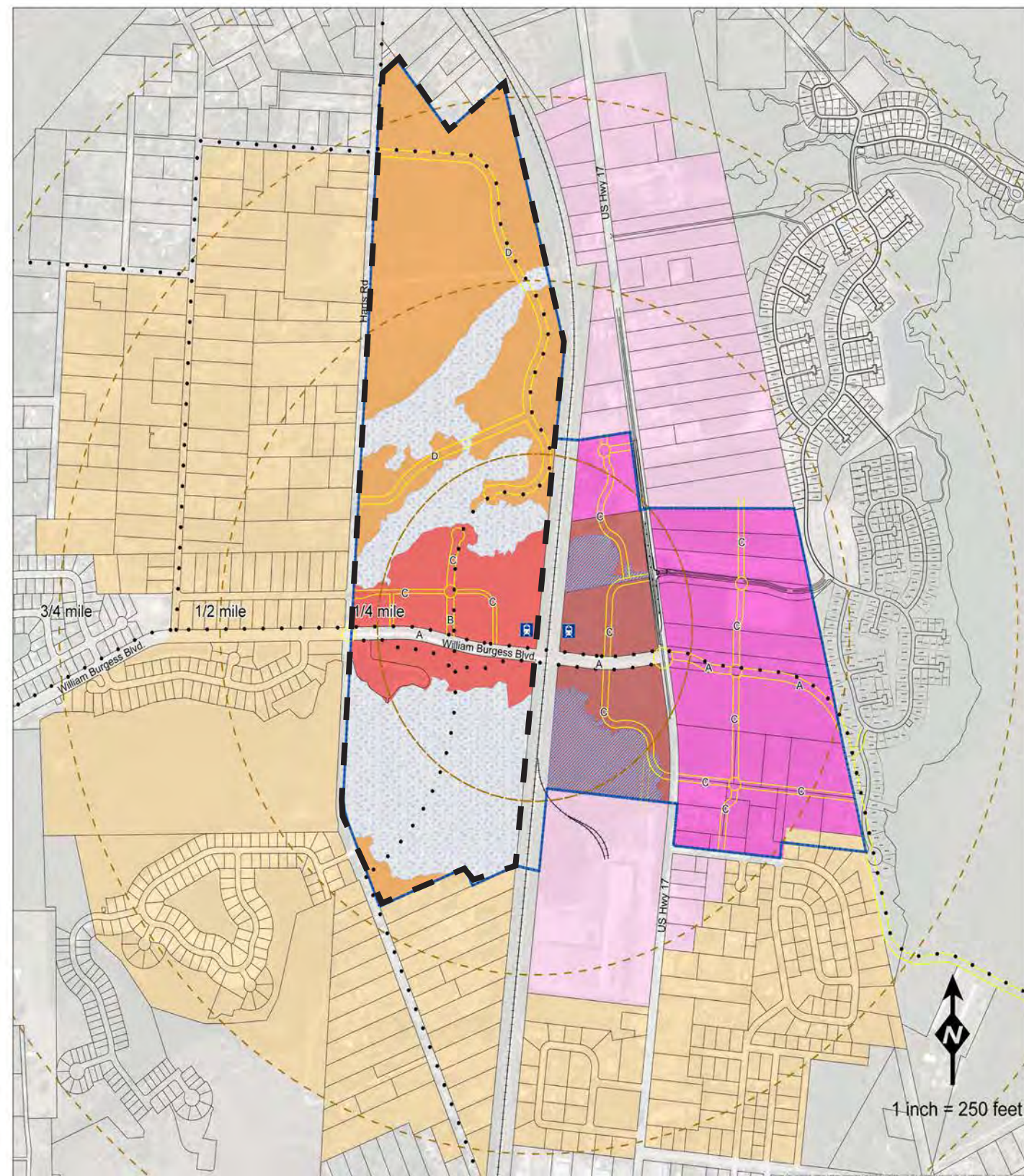


Water Tower at the
Bryce House, Bryceville, FL
(Nassau County, FL)



Putnam House in Palatka, FL

GENERAL CONDITIONS



William Burgess Mixed-Use Activity Center Overlay

Regulating Plan

June 28, 2017 - Update



William Burgess Mixed-use Activity Center Transect Acres

Existing	Potential
35 acres - Core 1	137 acres - Future Center
17 acres - Core 2	501 acres - Future Edge
86 acres - Center	
88 acres - Edge	
79 acres - Conservation	638 acres - Total
306 acres	

0 250 500 1,000 1,500 2,000 2,500 Feet

Street Type	Transect
A Boulevard	Core - 1
B Main Street	Core - 2
C Commercial Street	Center
D Neighborhood Street	Future Center
Multi-use Trail Spine	Edge
William Burgess A.C. Boundary	Future Edge
TOD Concentric Rings	Conservation
CSX Rail Line	South Parcel Wetlands
New Roadways	Commuter Rail Station

Nassau Crossing PUD

Figure 2. William Burgess Mixed-Use Activity Center Regulating Plan

The Nassau Crossing Planned Unit Development (PUD) consists of approximately 196.78 acres located to the south of SR 200/A1A, located east of Harts Road to the west of the CSX Rail Line, and to the west of US Highway 17 to the east with William Burgess Boulevard bisecting the property towards the southern end of the property in Yulee, Florida, see Figure 2 (left). The Nassau Crossing PUD will consist of up to three-hundred and fifty (350) single family residential units, four-hundred and fifty (450) multi-family residential units [of which sixty-seven (67)/15% of total multi-family units are to be affordable/workforce housing as defined herein], one-hundred and fifty thousand (150,000) square feet of office space, and one-hundred and fifty thousand (150,000) square feet of retail space. Allowable uses, density, intensities, design standards and development conditions are described in detail herein and in the William Burgess District Vision Book, adopted by Nassau County on December 11, 2017.

The Final Development Plan(s) implementing the Nassau Crossing PUD shall be consistent with William Burgess District Vision Book and the conceptual geometric arrangement and spatial distribution of densities and intensities as depicted in the Preliminary Development Plan, see Figure 17 and 18 on page 29 & 30. The implementation of this PUD shall be consistent with the adopted William Burgess District Vision Book and the design standards, themes and controls defined herein.

The William Burgess District is broken down into transect zones, which are based on the cone of walkability, in order to create a dense, mixed-use development. The “cone of walkability” is centered around the proposed transit station.

1. Village Core - Immediate Transit Oriented Development centered on the proposed commuter rail station and within a 1/4 mile to promote walkability, pedestrian orientation and social engagement. The Village Core will feature vertically integrated mixed-use buildings of higher density and intensity standards to promote a vibrant and active nucleus. Site design shall be at a pedestrian scale with buildings oriented to street. For the purpose of this PUD we will focus on Transect Core 1. Key elements for the Village Core include high density mixed use development comprised of commercial spaces, offices and apartments/multi-family housing, public spaces to promote common ownership and allow for interactive social spaces, common urban design characteristics, multi-modal transportation, and interconnected pedestrian networks.
2. Village Center - Begins near the 1/4 mile point and extends to approximately 1/2 mile from the proposed transit facility and commuter rail site and also features a pedestrian orientation with additional street types and parking configurations. The Village Center is also intended to be compact mixed use but at a lesser intensity than the Village Core to provide a transition to the Village Edge with a greater concentration of integrated housing types. For the purpose of this PUD there are no parcels designated as Village Center.
3. Village Edge - The Village Edge begins near the 1/2 mile point and extends to approximately 3/4 mile from the proposed transit facility and commuter rail site. The Village Edge has lower densities and intensities than the Core and Center. The Edge generally accommodates limited neighborhood scale commercial uses and housing in the form of the “missing middle” and the more traditional single family detached home. The Village Edge is still within the cone of walkability and connectivity is vital. Key elements within the Village Edge include diverse housing types, public spaces, urban design to encourage public activity along the streets, interconnected mobility networks including multi-use trails, and a variety of housing types.

The regulating plan also determines street types (see Page 23) , a connected roadway network throughout the entire overlay district, and an interconnected multi-use trail network to allow for multi-modal transportation throughout the corridor.

The Nassau Crossing PUD is intended to promote multi-modal transportation through the provision of transit facilities, integrated multi-use trail system, complete pedestrian/bicycle facilities, and reasonable automobile access. The single family residential portion of the proposed development located within the Edge Transect will have up to three vehicular access points to Harts Road. A multi-use trail running on a north-south axis will provide continuous connectivity from the northwesterly most entrance of the development to the south side of William Burgess Boulevard as depicted at Exhibit “C”. A multi-use trail will also be constructed along an east-west axis parallel and adjacent to William Burgess Boulevard. Sidewalks and bicycle facilities will be provided to connect the individual components of the development to the multi-use trail system, transit facilities, recreation areas, retail and office uses, and social spaces.

A significant percentage of the site will be reserved as public space for social and recreation purposes. These areas are differentiated from stormwater management facilities and wetland preservation areas. In Parcel B these public spaces take the form of greens, courtyards, squares, pocket parks, viable sidewalk zones that activate the street, and other similar components of urban design. Multi-use trails, tot-lots, fitness stations, pocket parks, walking paths and similar elements are to be designed into Parcel A. Parcel C will be dedicated to the public as a shared space to promote social, recreation and general community activities. This area will include the requisite improvements to support the intended use. Parking in Core 1 shall be to the side and rear of buildings.

Within the Nassau Crossing PUD, requirements defined in Section 33.01 of the Nassau County Land Development Code establishing a distance separation between vendors of alcoholic beverages and certain uses shall not apply. All other related provision shall apply. The theme, materials, application of materials and intent as defined in this PUD and the William Burgess District shall control. All development within the Nassau Crossing PUD shall demonstrate consistency with the materials, aesthetic controls, design guidelines, architectural design standards and intensities/densities defined in the Nassau Crossing PUD and the William Burgess District Vision Book.

PHYSICAL CHARACTERISTICS

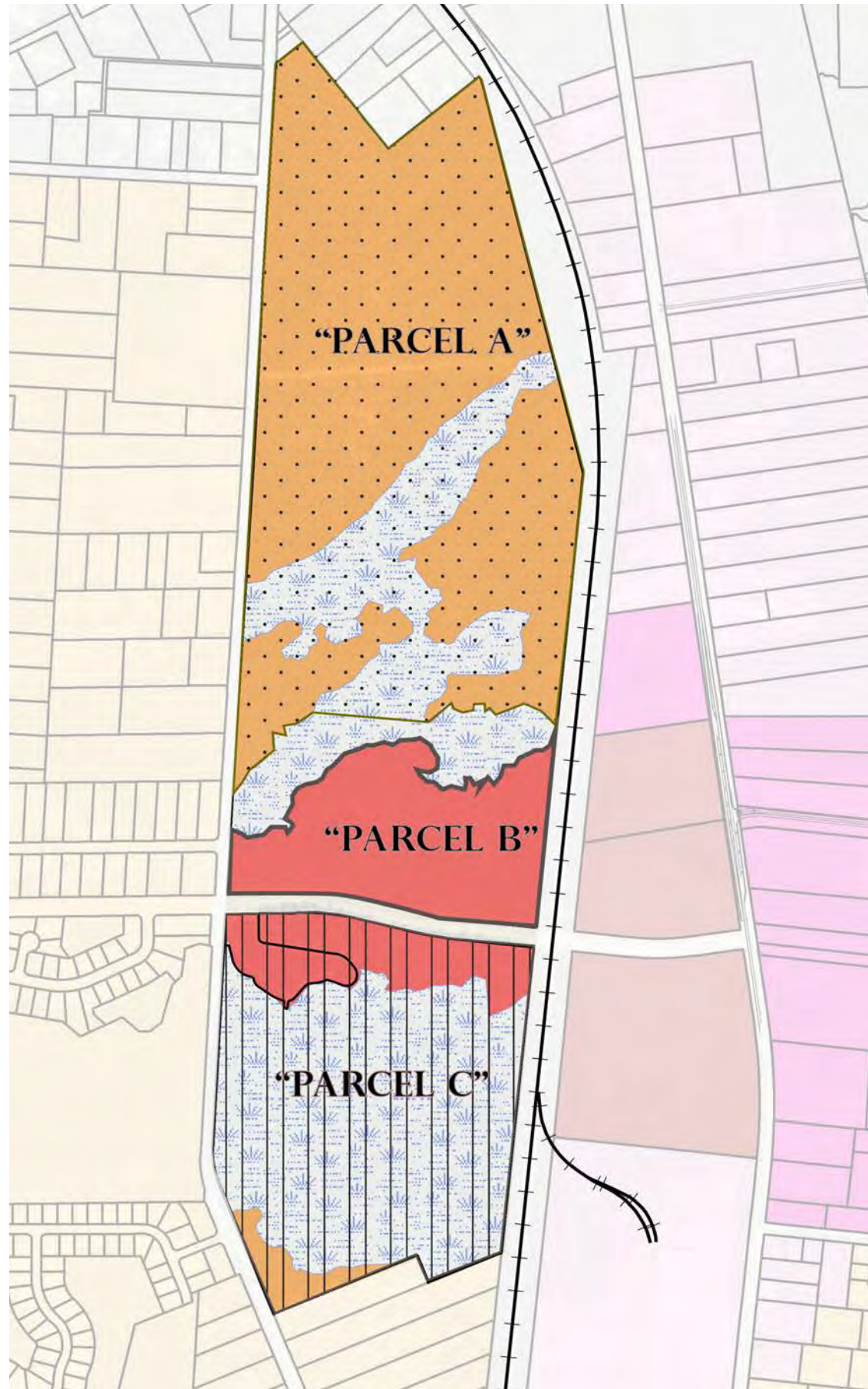


Figure 3. PUD Map

The Nassau Crossing PUD is divided into three parcels based on their transect zones, their future land use, and their designated development type (see Figure 3 - PUD Map).

A. Parcel A:

1. Transect - Village Edge
2. FLUM Designation - Medium Density Residential and Conservation 1
3. Primary Use - Residential

B. Parcel B

1. Transect - Village Core 1
2. FLUM Designation - Commercial and Conservation 1
3. Primary Use - Compact Mixed-use

C. Parcel C

1. Transect - Village Core 1 and Village Edge
2. FLUM Designation - Commercial, Conservation 1 and Medium Density Residential
3. Recreation

1. The site is predominantly pine flat woods with significant wetlands separating the single-family residential portion of the site (Parcel A/Village Edge) from the mixed-use area (Parcel B/Village Core I). The portion of the site south of William Burgess Boulevard is predominantly wetlands except for +/- 11.5 acres of uplands fronting on William Burgess Boulevard and a 3.5 acre parcel fronting on Harts Road at the southwesterly most extent of the development.

2. Soil conditions are not expected to pose any significant limitation on development outside of the jurisdictional wetlands.



PARCEL A (VILLAGE EDGE) INTENDED DEVELOPMENT



“Parcel A” (see Figure 4 - right) will be developed with up to 350 single-family residential dwelling units.

To promote a diversity of housing types and create a more interesting and dynamic development pattern, the applicant at his/her discretion may develop all or a portion of “Parcel A” with any of the following:

- » Single-family detached housing,
- » Duplex,
- » Triplex,
- » Quadplex,
- » Courtyard apartments,
- » Bungalow courts,
- » Pocket neighborhoods,
- » Town-homes, or
- » Live/work units.

Single family detached lots will also come in varying sizes as defined herein.

A change in the Lot layout and/or Lot type as depicted on the Preliminary Development Plan (Figure 17 and 18 on page 29 & 30) that does not result in more than 350 dwelling units in “Parcel A”, does not adversely impact the non-vehicular connectivity throughout the WBD, and is consistent with this PUD shall not require an amendment to the PUD. It is encouraged the Applicant explore a diverse product line and lot configuration.

The Final Development Plan for Parcel A shall be submitted for approval within one year of approval of the PUD or within such later date as is approved by the Board of County Commissioners. After initial approval of the Final Development Plan, changes, subject to approval by the Board of County Commissioners, may be made at any subsequent time.

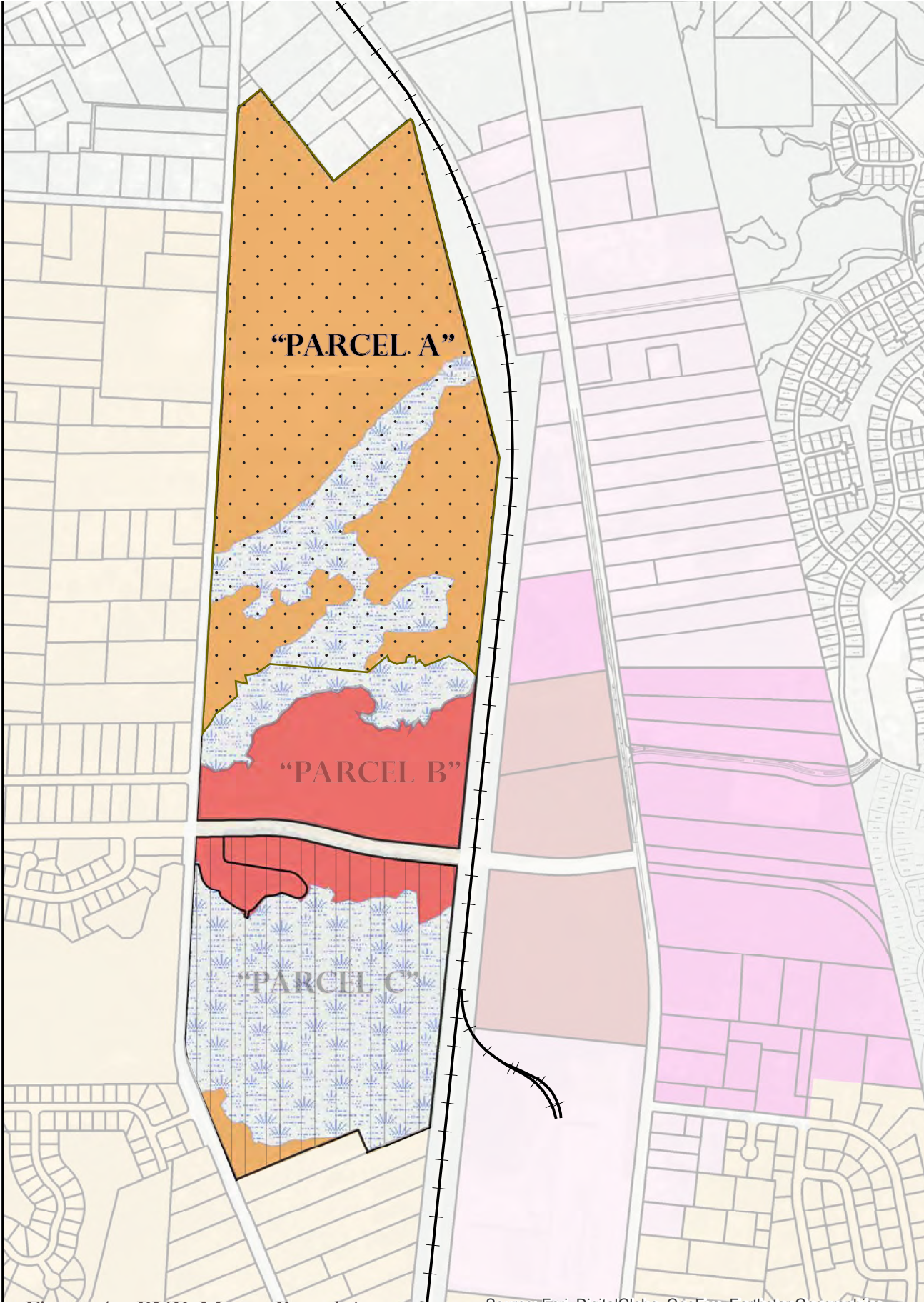


Figure 4. PUD Map - Parcel A

SINGLE FAMILY DETACHED

MINIMUM LOT AREA

The minimum lot areas are: 4,000 s.f., 5,000 s.f. and 6,000 s.f., as designated on the Final Development Plan.

MINIMUM LOT WIDTH

The minimum lot width is 40 feet for 4,000 square foot lots, 50 feet for 5,000 square foot lots, and 60 feet for 6,000 square feet lots, as designated by the Final Development Plan. This is to provide for a variety of home designs and lifestyles.

MINIMUM LOT FRONTAGE

80% of the minimum lot width. On curvilinear streets, the lot frontage may be measured at the front setback line. On cul-de-sacs the frontage shall be at least twenty-five (25) feet.

PEDESTRIAN WALKWAY

All Lots that have a sidewalk or trail running in front of the home shall provide a pedestrian walkway from the front porch to the sidewalk/trail.

MAXIMUM BUILDING HEIGHT

Forty-Five (45) Feet.

MAXIMUM LOT COVERAGE

For a 40-foot lot, the maximum lot coverage is 60%
For a 50-foot lot, the maximum lot coverage is 55%
For a 60-foot lot, the maximum lot coverage is 50%

Final Development Plan for each phase shall indicate that upon completion, no particular lot size shall comprise more than seventy percent (70%) of the total number of lots.

FRONT YARD SETBACK

A ten (10) foot minimum setback from the right-of-way to the lead vertical support of a front porches. Porches shall be required and shall have a minimum setback of 10’ and a maximum setback of 15’. * *
A fifteen (15) foot minimum front setback measured from the right-of-way to the lead vertical support of the main body of the home.
However, the front setback to the face of a garage shall be a minimum of twenty feet (20).
Front porches or faux front porches shall be required and shall be set back from the ROW a maximum of 15’. ***

SIDE YARD SETBACK

A five (5) foot minimum side setback on each side measured from the property line.

REAR YARD SETBACK

A ten (10) foot minimum rear setback measured from the rear property line.

CORNER LOTS

On corner lots the secondary frontage shall have a minimum setback of 10’. The side of the home facing the right-of-way for the secondary frontage shall use a combination windows, architectural elements and landscaping to soften the transition to the roadway. Landscaping/street trees shall be established to maintain a clear delineation between the public and private realm.

MINIMUM SEPARATION

A minimum ten (10) foot separation between structures shall be maintained. Eave encroachments up to 18 inches in all yards shall be allowed. Decks and patios thirty-six (36) inches or less as measured from grade may be located in a required side or rear yard.

A single family home may be located on a platted lot, a combination of platted lots, or a portion of a platted lot so long as the building parcel is at least as large as the minimum building parcel size, and the proposed construction meets all required setbacks, and the total number of units does not exceed the number of platted lots contained within that a particular plat.

Home Type	Lot Size (square feet)	Setbacks			Minimum Lot Frontage	Maximum Building Height	Maximum Lot Coverage
		Front/ <u>Street Side (max/min)</u>	Side (<u>min</u>)	Rear (<u>min</u>)			
Detached Single Family	4,000	10’ - 15’ Front Porch (<u>max</u>), 15’ Main house 20’ Garage (<u>min</u>)	5’ Each Side	10’	40’	45’	60%
Detached Single Family	5,000	10’ - 15’ Front Porch (<u>max</u>), 15’ Main house 20’ Garage (<u>min</u>)	5’ Each Side	10’	50’	45’	55%
Detached Single Family	6,000	10’ - 15’ Front Porch (<u>max</u>), 15’ Main house 20’ Garage (<u>min</u>)	5’ Each Side	10’	60’	45’	50%
<u>Townhouse, Duplex and Triplex</u>	<u>1,600</u> 2,000 (Interior) <u>2,600</u> 3,000 (Exterior)	10’ - 15’ Front Porch (<u>max</u>), 15’ Main house 20’ Garage (<u>min</u>)	0’ (Interior) 5’ (Exterior)	10’	18’ (Interior) 30’ (Exterior) Variable****	45’	70%
<u>Townhouse</u>	<u>Variable*****</u>	10’ - 15’ Front Porch (<u>max</u>), 15’ Main house 20’ Garage (<u>min</u>)	0’ (Interior) 5’ <u>Between Buildings</u> (Exterior)	<u>Variable****</u>	<u>Variable****, *****</u>	45’	<u>Variable*****</u>
Pocket Neighborhood*	750 - 2,500	Subject to Review	5’ Each Side <u>Between Buildings</u>	Subject to review	Variable****, *****	25’	80% <u>Variable***** & Subject to Review</u>

* Pocket Neighborhood plans must be approved by the Department of Planning and Economic Opportunity as they have specific design requirements including, but not limited to, communal spaces, shared parking areas and geometric arrangement.
**Alternative architectural elements or design measures may be substituted for the front porch.
***On curvilinear streets or cul-de-sacs the depth/setback of the porch or alternative architectural element/design measure may vary provided the intent is maintained.
****Frontage is typically on a walkway, path, green, courtyard or other similar common area.
*****Lot size may match the building footprint so as to allow greater communal areas for social and recreational purposes.

TABLE 1: LOT TYPOLOGY CHART

Parcel A (Village Edge Transect)

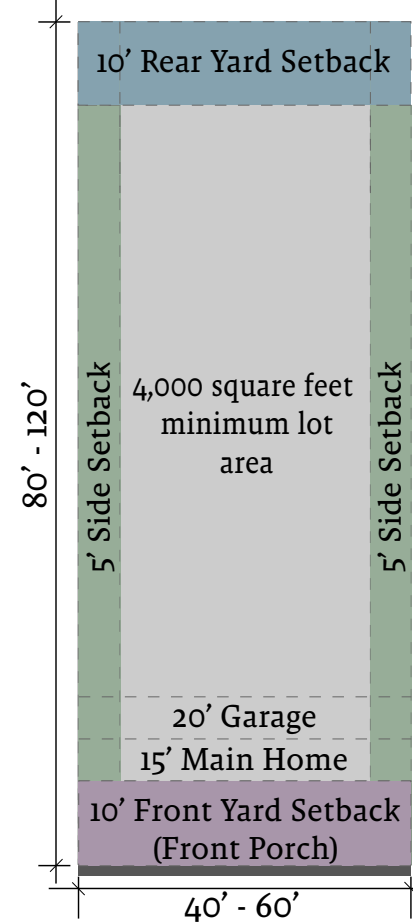


Figure 6. Single Family Detached Setbacks

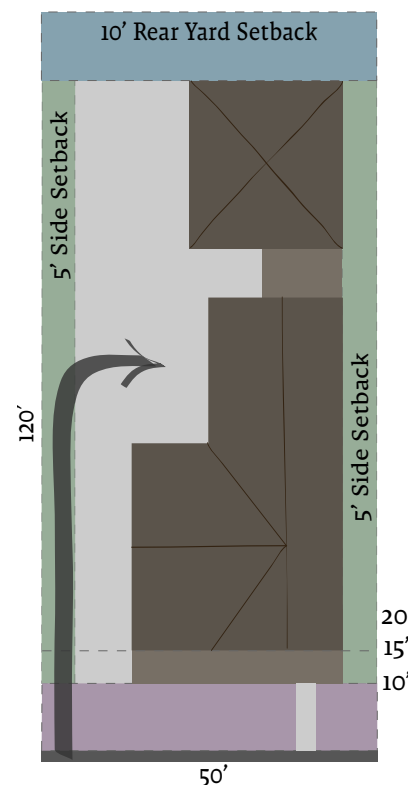
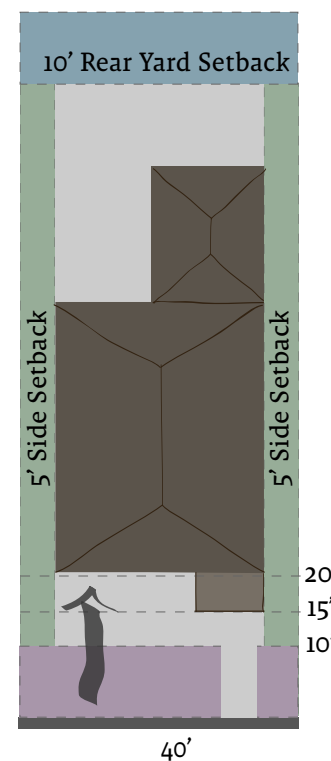
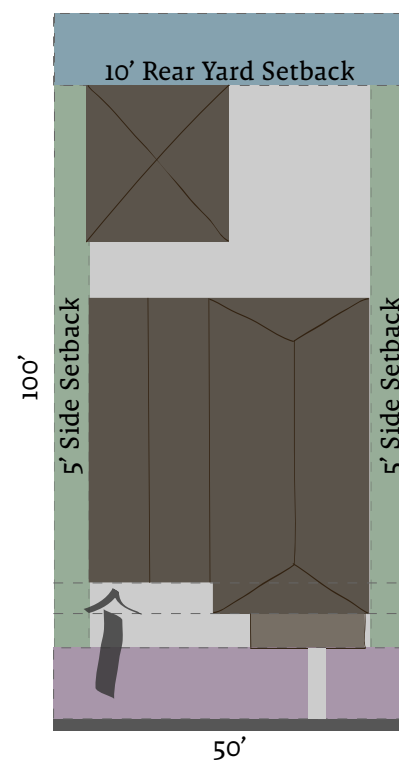
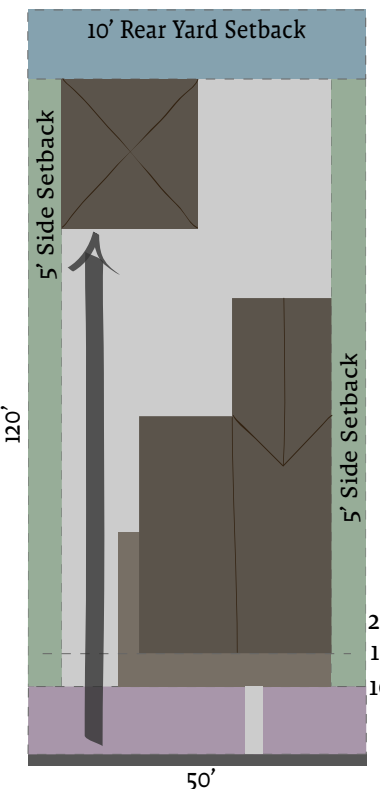


Figure 5. Single Family Detached Design Examples



Parcel A (Village Edge Transect)



TOWNHOUSE DESIGN EXAMPLES

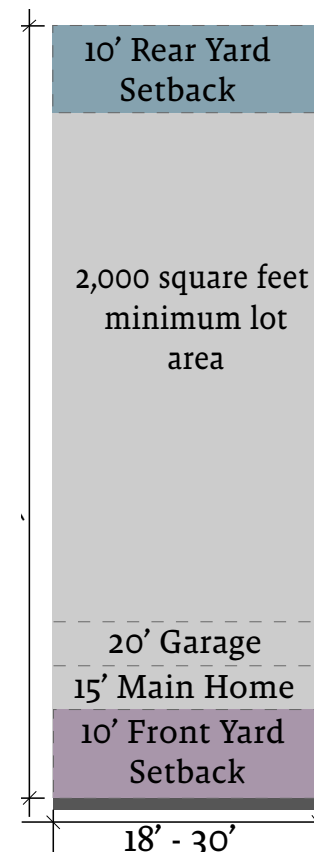


Figure 7. Townhouse Setback Example, See pg. 8 for specifics

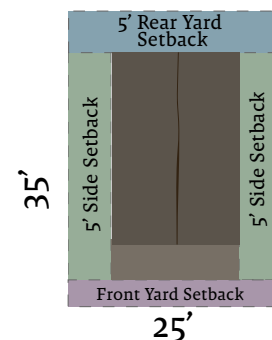


Figure 8. Pocket Neighborhood Design Examples

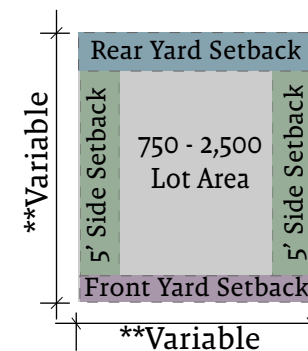


Figure 9. Pocket Neighborhood Setbacks

PROHIBITED ACCESSORY USES

Noncommercial greenhouses and/or plant nurseries and private boat/RV houses or shelters shall not be allowable uses. For example:



PERMITTED ACCESSORY USES

»Guest houses, accessory dwelling units, carriage houses, garage apartments. These units may have a separate electric meter and water service but shall be maintained under unified ownership. In other words, platted individual Lots cannot be further subdivided to create separate parcels of the accessory dwelling unit. It is encouraged that accessory dwelling units be utilized to accommodate multi-generation housing and diversity in housing stock.

»Mobile homes and model homes shall be allowed as temporary uses for sales centers and construction offices until such time as all of the residential units have been constructed and sold in the development.

»Home occupations in accordance with the provisions of Section 28.14 of the Land Development Code shall be the only allowable conditional uses within Parcel "A".

»Customary residential accessory structures as defined in Section 28.15 of the Land Development Code of Nassau County are permissible if not otherwise prohibited herein.



Setbacks:

- All screened pool enclosures, whether attached, semi-attached or detached from the principal building, shall adhere to a minimum yard setback requirement of five (5) feet and shall not be located between the street and principal structure.

PARCEL B (CORE 1) INTENDED DEVELOPMENT

“Parcel B” (see Figure 10 - right) shall be developed as a mixed-use compact node with an emphasis placed providing a healthy mix of residential, retail, office and recreational/social opportunity blended at a pedestrian scale offering a vibrant and sustainable live, work, play environment. It is the intent of “Parcel B” to serve as a socially engaged compact community centered on multi-modal transportation as intently differentiated from the predominant automobile oriented development pattern of the unincorporated areas of Nassau County. It is not the intent of Nassau County or the Applicant that “Parcel B” be developed as single-use development but rather a mix of complementary uses joined through sound urban design techniques and vertically integrated structures.

The entitlements assigned to “Parcel B” include up to four-hundred and fifty (450) multi-family units [of which sixty-seven (67)/15% of total multi-family units are to be affordable/workforce housing as defined herein, one-hundred and fifty thousand (150,000) square feet of office space and one-hundred and fifty thousand (150,000) square feet of retail space. The ratio of uses shall be set to ensure the mixed-use development pattern is executed. At build-out the minimum densities and intensities shall be provided:

- » Two-hundred and fifty (250) multi-family units with thirty-eight (38)/15% being affordable/workforce units.
- » Forty-five thousand (45,000) square feet of office space
- » Forty-five thousand (45,000) square feet of retail space

For the purposes of this PUD affordable/workforce housing shall be defined as eighty percent (80%) of Area Median Income (AMI). At least 50% of the units built in each phase shall be market rate units. Nothing herein prohibits the establishing affordable units at less than 80% AMI.

Development within “Parcel B” shall comply with all aspects of this PUD and the adopted WBD Vision Book.

The Final Development Plan for “Parcel B” shall not be fragmented. The Final Development Plan for “Parcel B” shall include all of “Parcel B”, the redevelopment of William Burgess Boulevard to meet the ‘Boulevard Street Type’ design standard, traffic circle at William Burgess Boulevard and Harts Road and “Parcel C” as necessary to depict integration and master plan for stormwater facilities. Approval of any FDP within the Nassau Crossing PUD and commencement of substantial construction shall vest the entitlements of this Order. Once vested, there shall be no time restriction binding the owner/applicant or successors in title to submit a FDP for Parcel B.

The square footage of the transit station is in addition to the square footages described above.

To the extent feasible, surface level storm water retention facilities will not be located with Parcel B. When feasible, storm water shall be conveyed to Parcel C. The Final Development Plan for Parcel B and Parcel C may be submitted separately. However, the Final Development Plan for either Parcel B or C must include the stormwater management system improvements necessary to support improvements.

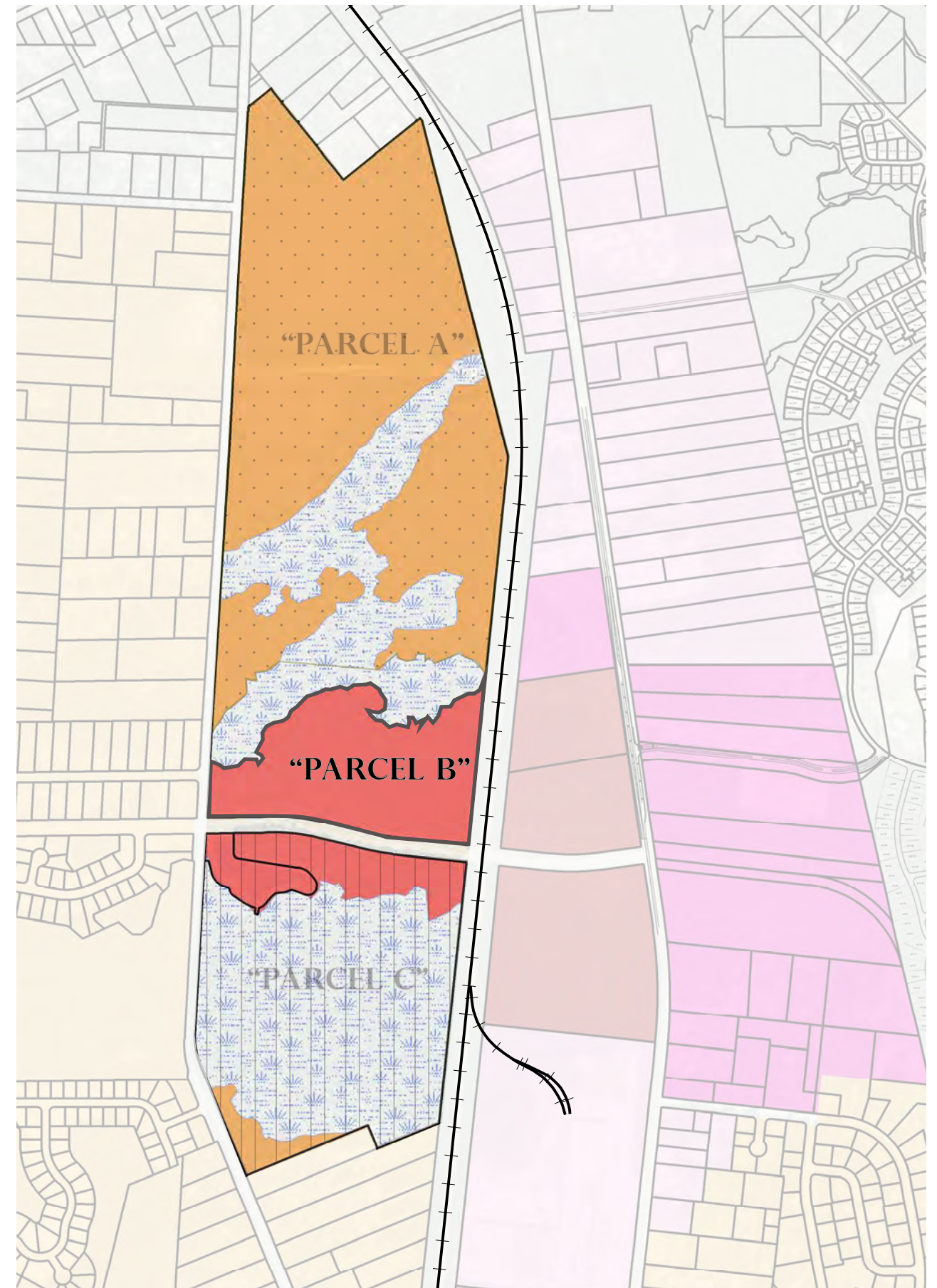


Figure 10. PUD Map - Parcel B

Parcel B (Core 1 Transect)

Design Guidelines/Standards: The intent of the Core I Transect is to promote compact, mixed-use development with integrated public spaces and an urban design character that is centered on multi-modal forms of transportation including bus transit and commuter rail in addition to strong pedestrian and bicyclist orientations. The following design guidelines/standards shall control development in “Parcel B”. The following are consistent with the adopted WBD Vision Book.

1. Maximum block size:

Block widths shall not be over two-hundred feet (200’). A pedestrian pass-through measuring at least 40’ in width that provides pedestrian access to a publicly accessible courtyard, square, green or other similar public space designed to encourage social activity and provide for a sense of shared space may serve to define a block. These public spaces are intended to be outside rooms that may be used for dining, entertainment, meeting space, pocket parks, community functions or any other similar use provided said space is available to the general public and is generally open and inviting.

2. Minimum Lot Requirements:

No minimum/no maximum

3. Building Restrictions:

(a) Width: In accordance with the maximum block size, single buildings shall have a maximum width of approximately two-hundred (200) feet. All buildings shall adhere to the architectural design standards defined herein and within the William Burgess Vision Book.

(b) Height:

i. Maximum height shall be sixty (60) feet or 5 stories.

ii. Minimum height shall be 2 stories. Faux 2nd stories are prohibited unless approved by the Planning and Zoning Board at a publicly noticed meeting. The Planning and Zoning Board shall determine the appropriateness of the request based on the intent of the William Burgess District.

(c) Floor Area Ratio (FAR)

i. Maximum FAR of two(2).

ii. Where residential and non-residential uses are included within the same structure, floor area ratio will exclude those portions devoted to residential dwelling units.

(d) Setbacks:

i. Front: 0’ – 15’(max 15’ from right-of-way/build to line at 15’). In other words, buildings shall not be setback from the ROW more than 15’. As depicted in Figures 17 and 18 on pages 29 & 30, in certain instances where the curvature of the roadway and the architectural styling results in a greater setback, reference the northeast corner of William Burgess Boulevard and the ‘Main Street’, the additional space may be used as an expansion of the sidewalk zone, courtyard, or similar publicly accessible social space. However, in no instance shall the additional setback adversely impact the character of the streetscape or design form.

ii. Side: 0’

iii. Rear: 0’

(e) A minimum of 85% of block width on the Boulevard and Main Street street types, shall be building frontage.

4. Lot Coverage:

(a) Maximum Impervious Surface Ratio(ISR): ISR will determined on a case-by-case basis. In no instance shall the impervious surface ratio exceed 90%.

(b) It is the intent that the impervious surface ratio will be viewed as an aggregate. In other words, the aggregate impervious surface area within the developable portion of Parcel B shall not exceed 90%.

5. Cross-access

(a) All non-residential, multi-family and mixed-use projects shall be designed to allow for vehicular cross access to adjacent non-residential, multi-family and mixed-use properties. Where there are stub-outs on adjoining properties, the site under review shall complete the connection. Where a vacant lot/tract of land with a Commercial, Industrial, High Density Residential or Multi-use FLUM designation is adjacent to the site under review, the cross access stub-out shall be constructed to the property boundary with the initial site development or appropriate phase of the project as determined by the Development Review Committee.

6. Utilities

See Page 25.

7. Signage

(a) See Page 26 and 37

(b) The examples provided on pages 26 and 37 shall control signage permitted within the Nassau Crossing PUD.

8. Lighting

(a) Lighting shall be consistent with the WBD and Nassau Crossing aesthetic/theme and applied through a unified lighting program adopted as part of the Final Development Plan for Parcel B and C. The unified lighting program shall be adopted with the first Final Development Plan for Parcel C or Parcel B

(b) Upon adoption of a unified lighting program for the WBD all lighting shall be consistent with the adopted lighting program.

(c) All sidewalks and multi-use trails shall be lighted to enhance usability and security subject to approval by the County Manager

9. Alcohol

Within the WBD and Nassau Crossing PUD, the requirements defined in Section 33.01 of the Nassau County Land Development Code establishing a distance separation between vendors of alcoholic beverages and certain uses shall not apply. All other related provision shall apply.

10. Trails, Streets and Pedestrian Facilities

(a) See pages 19, 20, 23 & 24 for additional detail.

(b) All new development and redevelopment shall provide pedestrian cross connectivity throughout the WBD in the form of sidewalks or multi-use trails.

(c) As approved by the Public Works Director of Nassau County, streets may be constructed at widths beneath the minimum standard defined in the Road Way and Drainage Standards of Nassau County to promote form and walkability.

(d) Narrow streets/travel lanes are encouraged. See page 24.

11. Public Spaces

All development within the Core 1 Transect shall provide public spaces in the form of greens, courtyards, squares, plazas or similar spaces designed to encourage social activity and provide for a sense of shared space. These public spaces shall be interconnected by orientation or pedestrian facilities. These public spaces are intended to be outside rooms that may be used for dining, entertainment, meeting space, pocket parks, community functions and other similar activities which are open to the public. It is the intent that the public spaces within Parcel B are linked to provide a vibrant interconnected District of activity.

12. Parking

(a) On-site parking shall not exceed 110% of the minimum requirement defined in Article 31 LDC.

(b) Within the Core 1 Transect the minimum parking requirements are 50% of that defined in Article 31 LDC.

(c) Parking requirements can be further reduced through the provision of on-street parking and shared parking facilities/agreements. Shared parking agreements and on-street parking are strongly encouraged. On-site surface parking is discouraged.

(d) The shared parking facility depicted within the land reserved for a Transit Facility is intended to be utilized to off-set parking within the Core Transect and is encouraged to be utilized by the entities located within the Core Transect, guests, transit riders and others. It is the intent of the Applicant and Nassau County the shared parking area be utilized in conjunction with all development within the Core Transect.

(d) All on-site parking shall be located behind or beside the primary structure and screened from the street. Parking shall not be located between the street and the primary structure. Parking lots shall not be located in any required yard (setbacks).

(e) To the extent possible, surface parking within the Core 1 Transect should be minimized to maintain the density/intensity standards and general form of the Transects. Parking shall be provided through on-street parking and shared parking facilities to the maximum extent possible.

(f) Parking areas will be separated and screened from public streets, sidewalks or rights-of-way using a landscaped area at least five (5) feet wide that includes a 3’ high wall and 2’ tall shrubs to screen public parking lots and service areas. Walls over 3’ in height shall be at least 50% transparent. This provision does not exempt a project from meeting the perimeter landscape buffer requirements defined in Section 37.05 LDC - see Section 15.

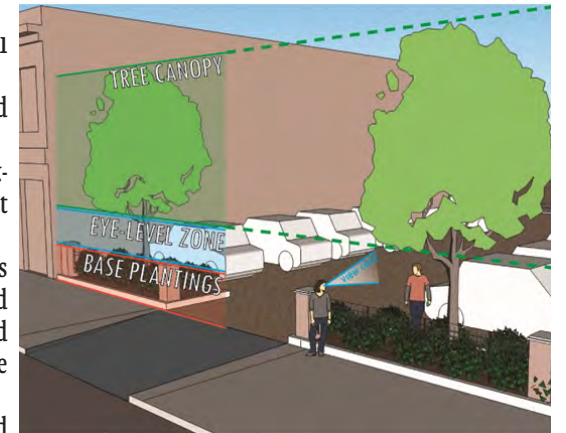


Diagram 1

