

## Parcel B (Core 1 Transect)

### 13. General Architectural Design Standards

#### (a) Building Orientation:

Buildings shall be oriented so as to enhance the appearance of the streetscape. It is the intent of these architectural standards to create interactive and permeable street level facades. The street and sidewalk zone shall be engaged and utilized as meaningful public space. This requirement shall be met by incorporating the following techniques into project design:

- The building's primary entrance(s) shall face the public streets and be oriented to the sidewalk zone. In the event that access is provided by two (2) or more streets, the building's primary façade shall face the street determined by the County to be the major street in addition to any primary pedestrian traffic entrance from common areas and dedicated public spaces.
- Buildings located at street intersections shall be designed to address the intersection and engage the street in an interactive manner. The primary entrance shall be at the corner or entrances shall be provided on each frontage.
- Corner buildings whether free standing or developed as part of a block shall be designed to visually accentuate the vehicular and pedestrian experience. These elements shall be designed to exemplify the aesthetic of the overall WBD theme. It is not intended these features to be developed as individualistic signage.
- Each façade that is visible from a street or public area of adjoining properties shall be designed with full architectural treatment oriented towards the scale of the pedestrian and engaged with the sidewalk zone. Such treatments shall be consistent with the design requirements of this section and shall incorporate door and window placements, façade architectural treatments and detail, roof design and building material applications necessary to give the appearance that each visible façade is a primary façade oriented towards the pedestrian.
- The architectural treatment requirements defined herein shall also be applied to any building façade which is situated where it is visible from a street or public space of an adjoining building.
- Building orientation shall be such that service areas are placed out of view from a street and adjacent properties. Blank walls or service areas are not allowed at frontages.
- Ornamental and structural architectural details, such as bays, columns, gables, belt courses, lintels and pilasters shall be applied.



Do This



Not This



Do This



Not This

#### (b) Building Transitions:

Façade and height transitions between buildings are key elements in creating and maintaining an attractive streetscape. Height and scale of a new development and redevelopment shall be compatible with that of surrounding development, provided such surrounding development complies with the standards set forth in this section. The following transitional techniques shall be applied to new development and redevelopment when within three hundred (300) feet of an existing building.

- Buildings shall be designed to provide transitional elements and architectural features that are architecturally compatible with adjacent structures. Buildings that are twice the height, or greater, than an adjacent structure shall also provide transitional elements and features that provide for transitional blending of heights.
- The pattern of placement, proportions, and materials of windows and doors shall be harmonious with surrounding structures.
- The ratio of wall surface to openings and the ratio of width and height of windows and doors shall be consistent and compatible with surrounding structures.

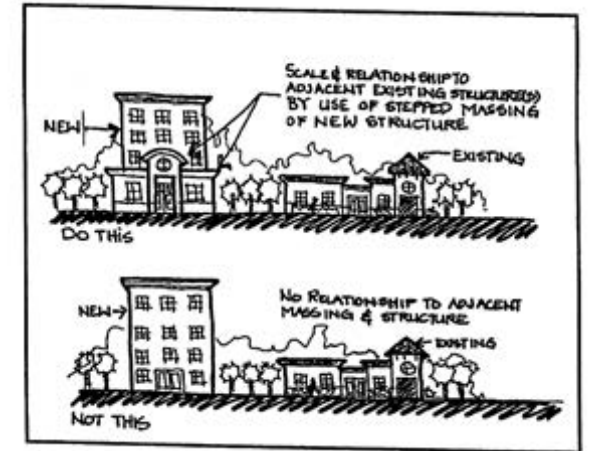


Diagram 2

#### (c) Exterior Materials and Colors:

Exterior building materials and colors contribute significantly to the visual impact of a building on a community, which, individually and collectively reflect upon the visual character and quality of a community. In order to project an image of high quality aesthetics, building materials and colors shall conform to the following requirements:

- The exterior design of all new structures must incorporate at least two (2) of the following elements:
  - Color change
  - Texture change
  - Material change
  - Pattern change
  - Architectural banding
- Exterior facing materials shall be consistent with the materials listed and applied in the WBD Vision Book and this PUD on all facades that are, or will be, exposed to the general public. Vinyl siding and opaque or reflective glass (or similar) shall not be permitted. Corrugated metal shall only be used as a facade accent as depicted in the examples herein and not exceed 10% of the façade without approval of the Planning and Zoning Board.
- Building materials and colors shall be consistent around the entire building. Exceptions to this provision may be made for portions of a structure that are not exposed to the general public.



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### (d) Roof design:

Roofs are an integral part of building design and shall be designed and constructed to add interest to and reduce the massing of buildings. Roofs shall incorporate the design elements listed below.

- The design of roof structures shall be of hip, gambrel, gable, skillion/lean-to, shed, jerkinhead and true mansard styles and shall be extended to all sides of the structure. Roof-like appurtenances such as false roofs, parapets and other similar features may be allowed only if such features are required for mechanical equipment screening or acoustical control that cannot be accomplished through utilization of approved roof styles.
- Application of such roof-like features shall be accomplished in such a manner as to minimize the appearance of a flat roof design. Roofs shall be designed to be of such height, bulk and mass so as to appear structural even when the design is non-structural.
- If flat roofs are utilized, the roof shall be surrounded on all sides by a continuous parapet wall and shall have the bulk and mass so as to appear structural in nature. In no instance shall the parapet height exceed 1/3 of the supporting wall height.
- The roof edge, where visible from any street or common area, shall have, at a minimum of two (2) locations, a vertical change from the dominant roof-line. Such change shall be a minimum of three (3) feet.
- Towers are recommended on buildings which terminate street vistas.



Do this

Notice the difference between the roof elements that are of a scale and mass to appear structural in nature. The example to the right fails to provide roof elements of a mass and scale to appear structural.



Not this

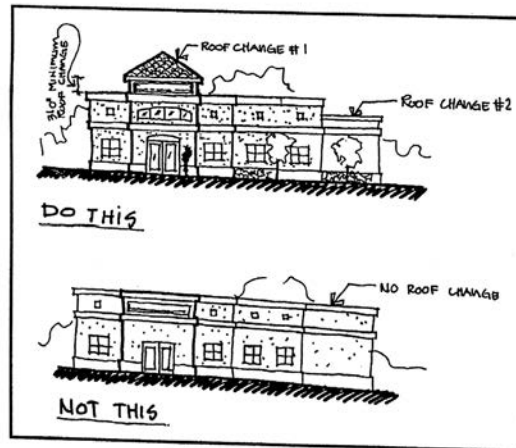


Diagram 3



Do this

Roof elements are of a mass and scale to appear structural.

### (e) Windows/Transparency and Entryways: Windows and entryways shall be designed in accordance with the techniques listed below:

- For all buildings fronting a street with non-residential uses on the ground floor, a minimum of 60% of the area between 2' and 8' vertically shall be transparent. For stories above the ground that contain non-residential uses, a minimum of 25% of the facade shall be transparent.
- Windows shall be recessed or shall project at least two(2) inches and shall include prominent sills, shutters, relief or other such forms of framing.
- Entryways shall be differentiated from the remainder of the façade through the use of color, change in materials, application of architectural features (arches, columns, colonnades, etc.), setbacks, offsets, arcade or gallery.
- Entryway design shall incorporate hardscape features such as low walls, decorative paving, water features and the like.
- Entryway areas shall be provided with structural or vegetative shading features and benches or other seating components are encouraged.
- All glass shall be clear, not dark or reflective.
- Solid security gates or roll-down security window/door covers are prohibited.

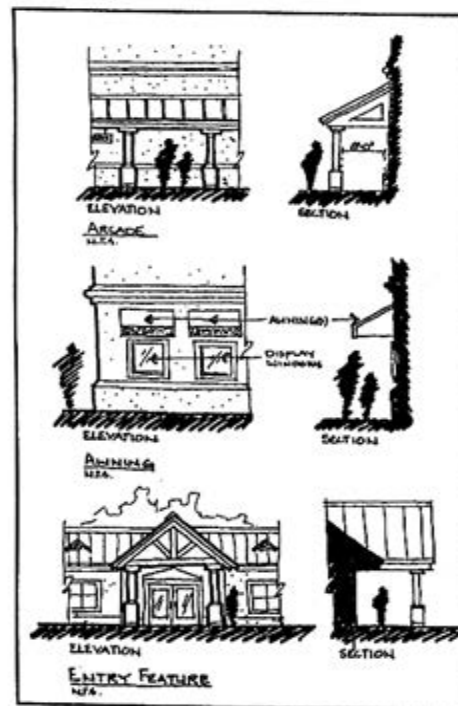


Diagram 4

### (f) Projections, Recesses and Massing:

Projections, recesses, and massing: In order to eliminate unadorned big box development and create structures and facades oriented towards the scale of the pedestrian, new development shall conform to the following requirements:

- The exterior design of all new structures must incorporate at least four (4) of the following features:
  - (1) Canopies or porticos
  - (2) Arcades (8 ft. minimum width)
  - (3) Raised cornice parapets over windows, doors, roofline
  - (4) Peaked roof forms
  - (5) Arches
  - (6) Clock towers, bell towers, cupolas and the like
  - (7) Reveals, offsets or projecting ribs, through a change in plane of no less than twelve (12) inches in width
  - (8) Building setbacks or projections, a minimum of three (3) feet in width on upper level(s)

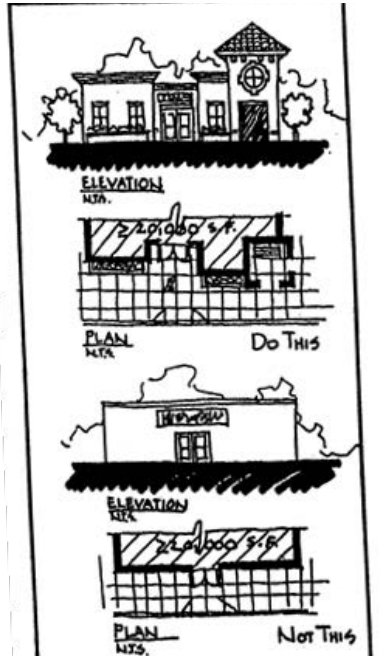
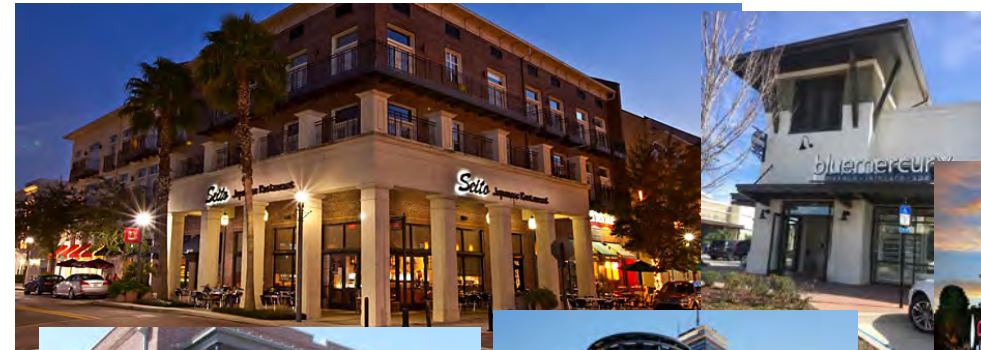


Diagram 5



Do this

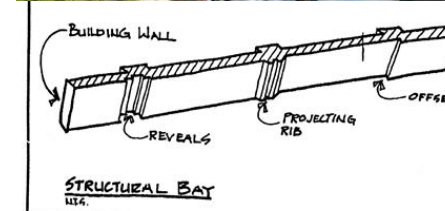
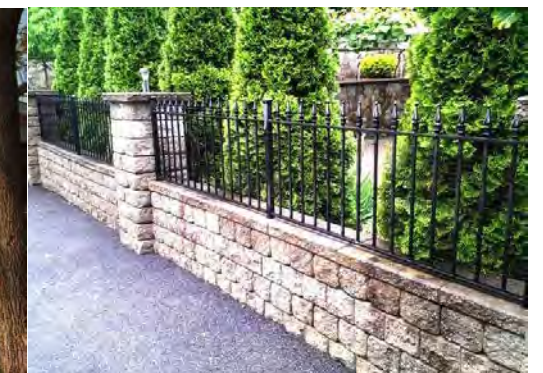


Diagram 6



### 14. Section Walls and fences

- Walls over 3' in height shall be at least 50% transparent. This provision is not applicable to equipment, dumpster and other similar service/operations equipment screening.
- All retention and detention facilities shall either be sloped as to not require fencing or, be bulkheaded (or similar) and include a stylistic fencing and incorporated in to the design of the site.
- Chain-link fencing is prohibited.



## Parcel B (Core 1 Transect)

### 15. Landscaping and Streetscaping (see also page 27):

- All sites shall meet the minimum standards for landscaping defined in Sec. 37.05 of the Nassau County Land Development Code unless expressly stated otherwise in the Nassau Crossing PUD or WBD Vision Book. The Planning and Zoning Board may waive or modify aspects of the landscape requirements when demonstrated to be directly implementing the supplemental design guidelines and intent of the WBD Vision Book pursuant to subsection 15 on page 15 of the Nassau Crossing PUD. The Planning and Zoning Board will consider the intent of the WBD, the relationship of the building/landscape material to the street, and impact to other properties. Financial implications on the applicant/site developer shall not be considered.
- A streetscape plan shall be submitted with each application for development. The streetscape plan shall depict the sidewalk zones and correlating features such as, but not limited to, street furniture, street trees, pedestrian areas and on-street parking. Native canopy trees shall be provided along all streets spaced no more than thirty (30) feet on-center. See the typical cross sections and sidewalk zones provided for on page 23 Figure 16.
- Native canopy trees shall be planted along pedestrian and bicycle facilities at a spacing of no more than thirty (30) feet on-center to provide shading and define the non-automobile spaces.
- Where possible and appropriate, landscaping should be native and wild in expression allowing the natural/native under-story to flourish. Incorporating swaths of natural vegetation into a project design is encouraged.
- All landscaping shall have permanent irrigation.
- Building walls that are 25' in length with no windows or a pedestrian pass-through shall utilize landscaping, in addition to architectural elements described in the Nassau Crossing PUD and WBD Vision Book, to soften the facade.
- Landscaping shall be placed on the outside of any wall to provide visual interest.
- The use of planters, tree wells and other similar elements of urban design shall be incorporated into the landscape program.
- All public/social spaces, including but not limited to, greens, courtyards, squares, etc, shall incorporate landscape programs, in addition to architectural element such as pergolas, into the design in such a manner that the space is aesthetically inviting and provides shade to encourage use.

### 16. Variations, Waivers and Interpretation

#### (a) Variations and Waivers

Request for variations and Waivers to the WBD supplemental design guidelines and the design guidelines and standards defined in the Nassau Crossing PUD shall be considered by the Planning and Zoning Board. Request for a waiver or variation shall be submitted to the Department of Planning and Economic Opportunity. The request for a waiver or variation shall include the material necessary for Planning and Economic Opportunity staff to review and make a recommendation to the Planning and Zoning Board. The request shall, at a minimum, induce the following:

- Thorough narrative describing the nature of the request and reason the request is necessary to make reasonable use of the land/building/site in question. the site plan shall incorporate enough of the WBD to provide context for the requested variation or waiver.
- Site plan. A site plan shall include the subject property and the necessary areas of the surrounding lands to determine the interrelationship of the WBD.
- Building elevations.
- Facade treatment details.
- Material list.
- Streetscape plan
- Landscape plan
- Any other material deemed necessary by PEO staff or the Planning and Zoning Board to adequately review and make an informed decision as to the request.

Upon receipt of a complete packet, the Planning and Zoning Board shall consider the request for a variation or waiver within 90 days of the request being made, or the first regularly scheduled meeting thereafter. The meeting at which the Planning and Zoning Board hears the request shall be publicly noticed. The Planning and Zoning Board shall base their decision on the following:

- A variation or waiver to these design guidelines and standards shall only be issued upon a finding the request maintains the intent of the WBD as described in the WBD Vision Book and the Nassau Crossing PUD.
- The variation or waiver does not compromise the development form defined in the WBD supplemental design guidelines, WBD Vision Book or Nassau Crossing PUD.
- The variation or waiver does not adversely impact adjacent properties or the relationship of the building/site to other buildings, development sites, public spaces or interaction/engagement with a street.
- The variation or waiver does not adversely impact mobility, access to other sites, amenities, public spaces or transportation options.
- Financial implication on the owner/developer is not a criteria of consideration by the Planning and Zoning Board.

#### (b) Interpretations

Questions of interpretation, intent and application of the design guidelines and standards defined in the WBD Vision Book and Nassau Crossing PUD shall be addressed to the Planning and Zoning Board. In cases of conflict, the Planning and Zoning Board shall have authority to render determinations of consistency, or lack thereof, with the WBD Vision Book and Nassau Crossing PUD.



Diagram 7



Diagram 8



Diagram 9

## PARCEL C/CORE 1, VILLAGE EDGE INTENDED DEVELOPMENT

“Parcel C” (see Figure 11 - right) measures approximately fifty-eight (58) acres, 15 acres of uplands, and is intended to serve as publicly accessible social and recreation space. The guiding design principle is the creation of social space in the form of outdoor rooms and inclusive amenities that serve as an extension of the public and social space provided within “Parcel B” and facilitate day-to-day social interaction. The improvements to “Parcel C” are intended to be engaging and serve as the social nucleus of the development and provide space for recreating, festivals, community events, farmers markets and the-like.

Improvements will include those necessary to facilitate the intent of “Parcel C”, such as, but are not limited to a combination of:

- » Multi-use trails, boardwalks, nature trails, and viewing platforms;
- » Manicured and irrigated grassed lawns to support free play, community events/festivals and informal field sports, multi-use space for festivals, farmers markets and community events;
- » Children’s play equipment;
- » Restrooms;
- » Picnic areas;
- » Gazebos;
- » Frisbee golf;
- » Outdoor fitness stations,
- » and other similar improvements.

Surface parking is prohibited within the boundary of Parcel C. All parking to support Parcel C shall be in the form on on-street parking consistent with the Boulevard Street Type defined in the street types exhibit on page 23 figure 16 and conceptual design depicted in the Preliminary Development Plan. Provided however, prior to the redevelopment of William Burgess Boulevard, parking may be provided in grassed areas on Parcel C and/or grassed or paved areas on Parcel B. Compliance with the Florida Accessibility Code/Americans with Disabilities Act will be addressed at the time of construction.

Development within Parcel C shall comply with all aspects of the PUD and the WBD Vision Book.

The Final Development Plan for “Parcel C” shall be submitted for approval within one year of approval of the PUD or within such later date as is approved by the Board of County Commissioners. After initial approval of the Final Development Plan, changes, subject to approval by the Board of County Commissioners, may be made as deemed appropriate.

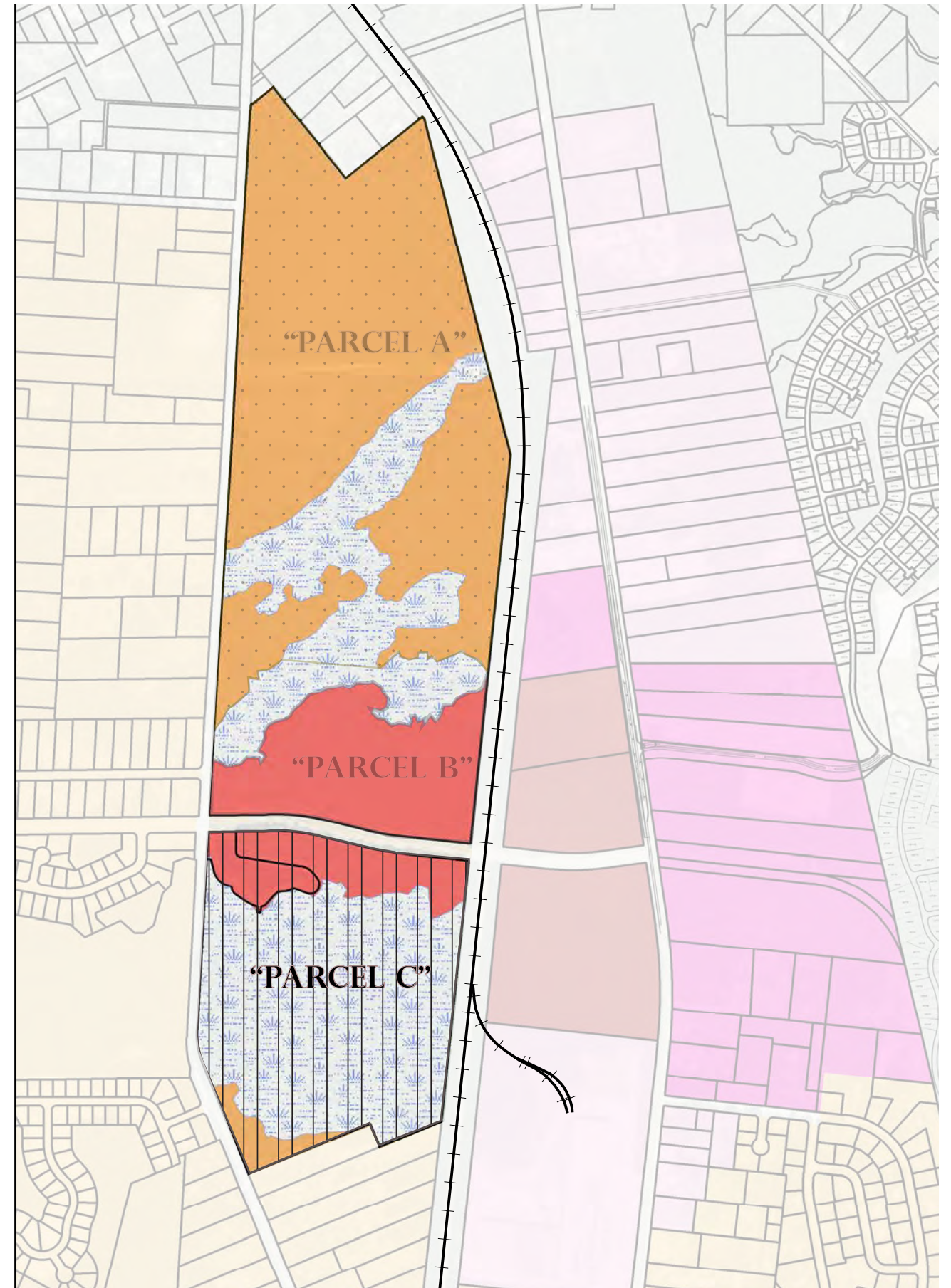


Figure 11. PUD Map - Parcel C

# RECREATIONAL AMENITIES

## GENERAL

1. The time frames specified herein for completion of the recreational improvements in “Parcel C” shall be the later or ten (10) months after the Applicant has received approval from Nassau County for the plans for the improvements required to be constructed by the applicant or the time specified in the Phasing and Timing section on the following page.
2. For Phase 1 improvements in Parcel C, the Site Engineering Plan for the correlating improvements in Parcel C shall be approved by the Development Review Committee of Nassau County prior to the Board of County Commissioners approving the first final plat within the Nassau Crossing PUD. For all other phases, The improvements triggered in the phasing schedule defined in this PUD shall be included with the correlating Final Development Plan and Site Engineering Plan which causes the respective threshold to be tripped.
3. To off-set impacts to the public park system and meet the intent of the development program the Applicant shall provide public recreational amenities as described in this PUD and the conceptual design of Parcel C as depicted in Figure 12 on page 18.
4. If no improvements are made within Parcel C within a ten (10) year period from approval of the Nassau Crossing PUD, Ownership of Parcel C shall revert back to the Applicant.
5. Nothing herein shall prohibit contributions by other developers within the WBD from constructing or causing to be constructed amenities within the park.
6. Parcel C shall be dedicated via an instrument approved by the County Attorney prior to the Board of County Commissioners approving the first final plat within the Nassau Crossing PUD approval of the first Final Development Plan for Parcel A. The instrument conveying title to the land shall include provisions which limits the use of Parcel C to public recreation and community based social purposes. A separate agreement between Nassau County and the Applicant will be executed prior to the Board of County Commissioners approving the first final plat within the Nassau Crossing PUD Final Development Plan which includes a portion of Parcel C. The agreement will address the ten (10) year reverter clause and management/maintenance responsibilities and development limitations of Parcel C.
7. Within Parcel A, the Applicant may construct or cause to be constructed tot-lots, picnic areas, gazebos, manicured lawns for free play when combined with benches, shelters and walking paths and/or any other recreational amenity for private use of residents with Parcel A.

## EXAMPLES OF PARK AMENITIES



## PHASING AND TIMING

- A. Phase I
- I. Phase I is defined as the earlier of the following:
    - a. The issuance of the 100th Certificate of Occupancy, or,
    - b. 100th multifamily unit is given a Certificate of Completion, or,
    - c. 30,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - II. Prior reaching the above threshold the following shall occur:
    - a. The Applicant shall clear, grub, level, seed or sod, and irrigate the open field area located in Parcel C and identified in the conceptual design of Parcel C (Figure 12).
    - b. Provide a temporary, handicap parking area, and grassed parking area in proximity to the open play field. The location of the parking will be decided at the time of construction. Nothing herein shall prevent Nassau County from using unimproved areas within the William Burgess right-of-way for parking or in Parcel B with the Applicant's agreement. The driveway shall meet the requirements of the Roadway and Drainage Standards and applicable ADA requirements.
    - c. Within Parcel C, as generally depicted in Figure 12, the Applicant shall construct or cause to be constructed a children's play area and a tot lot (with age separation). All improvements are to be consistent with the WBD/Nassau Crossing aesthetic/theme.
- B. Phase II
- I. Phase II is defined as the earlier of the following:
    - a. The issuance of the 200th Certificate of Occupancy, or,
    - b. 200th multifamily unit is given a Certificate of Completion, or,
    - c. 60,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - II. Prior reaching the above threshold the following shall occur:
    - a. Within Parcel C, as generally depicted in Figure 12, the Applicant shall construct or cause to be constructed a children's play area and a tot lot (with age separation), bicycle racks, restroom facilities, water fountain, at least one covered gazebo/structure. All improvements are to be consistent with the WBD/Nassau Crossing aesthetic/theme.
    - b. Completion of the internal looping trail system within Parcel C as depicted in Figure 12.
- C. Phase III
- I. Phase III is defined as the earlier of the following:
    - a. The issuance of the 350th Certificate of Occupancy, or,
    - b. 250th multifamily unit is given a Certificate of Completion, or,
    - c. The issuance of the final Certificate of Occupancy based on the final phase of the final recorded plat in Parcel A, or,
    - d. 100,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - II. Prior reaching the above threshold the following shall occur:
    - a. The Applicant shall construct or cause to be constructed two youth soccer fields and associated supporting infrastructure as depicted in Figure 12. The play fields shall be elevated and crowned to create a level and dry playing surface. The fields shall be constructed with a crown running along the center line, goal to goal, at a slope between 1% and 1.75%.
    - b. A series of at least five outdoor fitness station within the area depicted in Figure 12.
    - c. At least one(1) additional gazebos/covered area and three (3) picnic stations.
    - d. Complete the trail system depicted in Figure 12. In lieu of completing the trail system in the southwest upland area of Parcel C, the Applicant may make improvements of equal value to the plaza area as depicted in Figure 12. The Planning and Economic Opportunity Department shall have the administrative discretion to approve the proposed improvements to the plaza area in lieu of improvements in the southwest upland portion of Parcel C
- D. Phase IV
- I. Phase IV is defined as the earlier of the following:
    - a. The point in time, regardless of the above phasing schedule, when William Burgess Boulevard is redeveloped in accordance with the Boulevard Street Type as defined herein and within the WBD, or,
    - b. 350th multifamily unit is given a Certificate of Completion, or,
    - c. 150,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - II. Prior reaching the above threshold the following shall occur:
    - a. The Applicant shall clear, grub, level, seed, sod and irrigate the outdoor social space as depicted in Figure 12.
    - b. Shall construct viewing platforms as depicted in Figure 12.



JOHNNY CARSON PARK  
BURBANK, CA

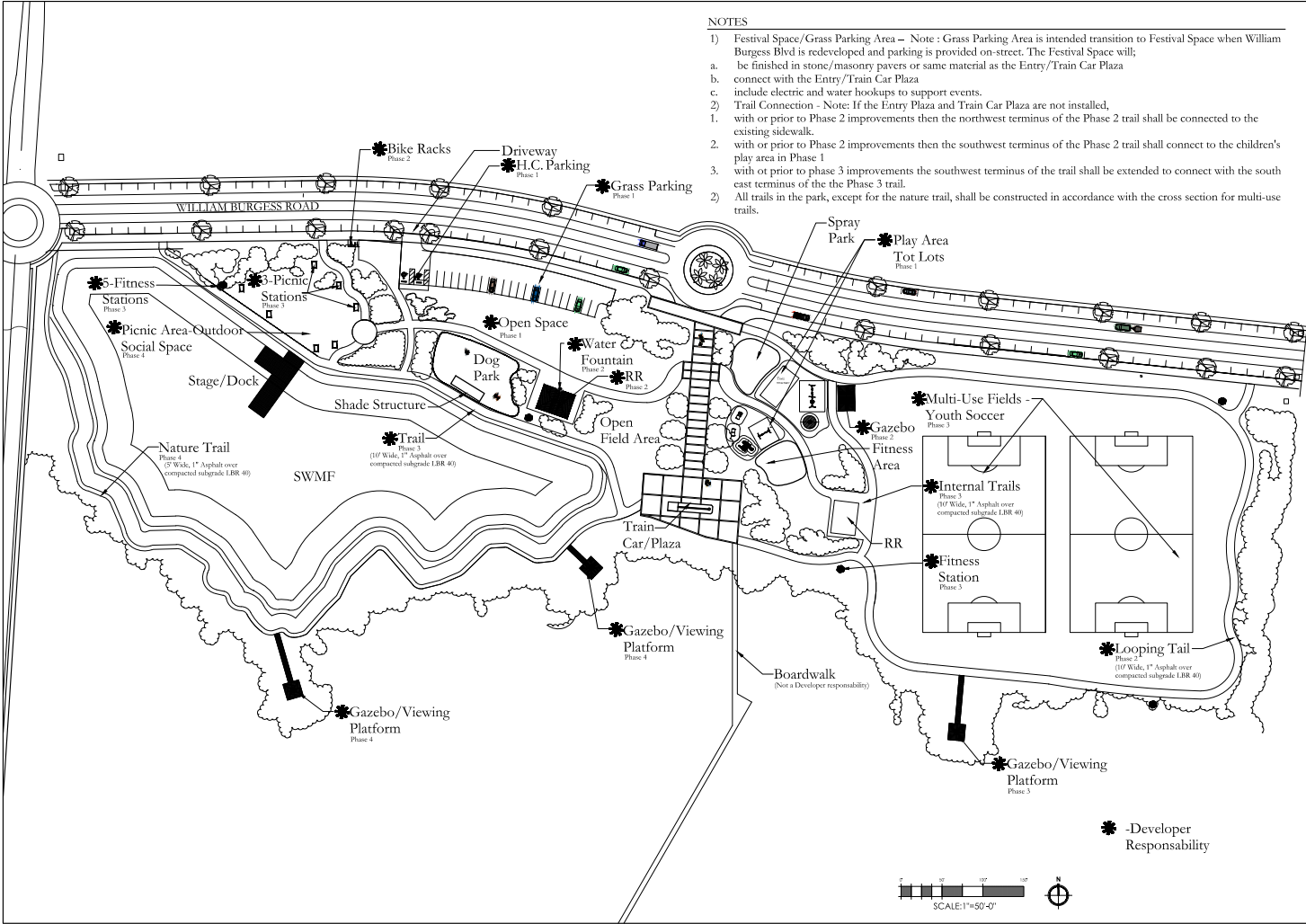
Lands within utility corridors can be utilized as amazing neighborhood parks and social space. Burbank, CA has many excellent public parks within utility corridors



WHITNALL HIGHWAY PARK  
BURBANK, CA



Park Design



PHASING FIGURE EXAMPLES OF PARK AMENITIES

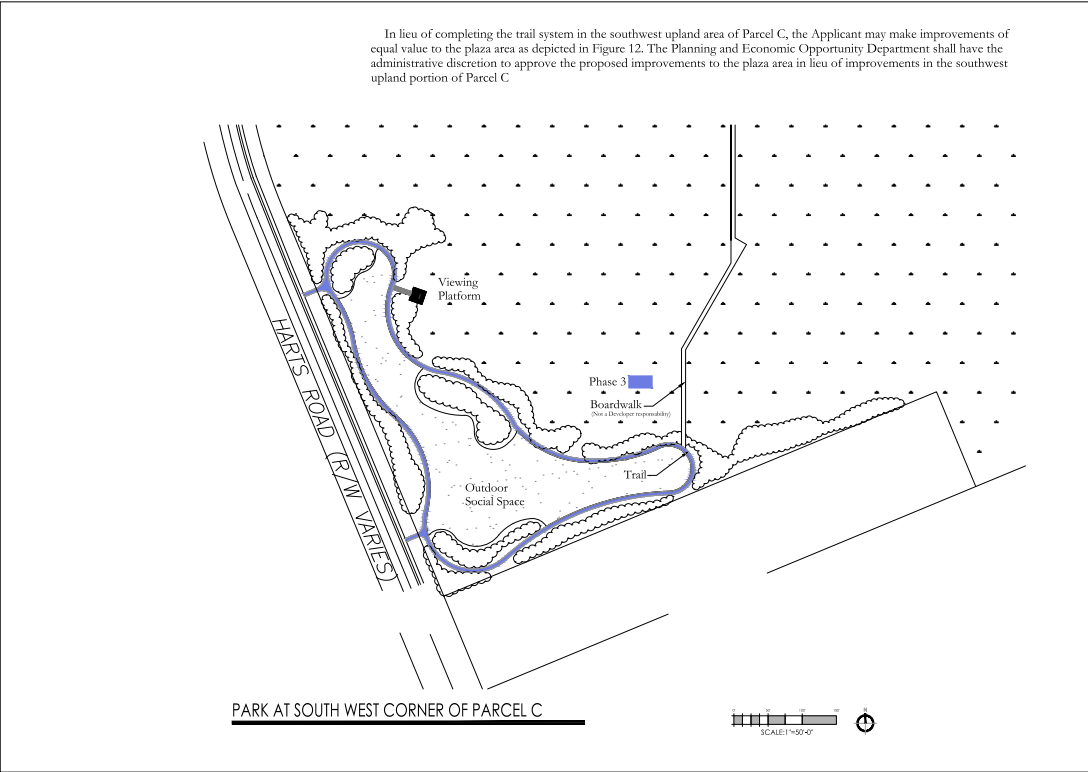
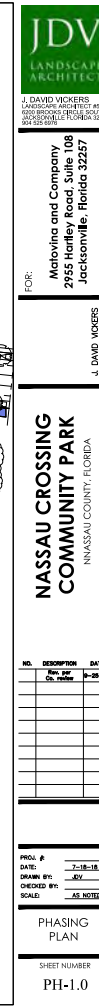
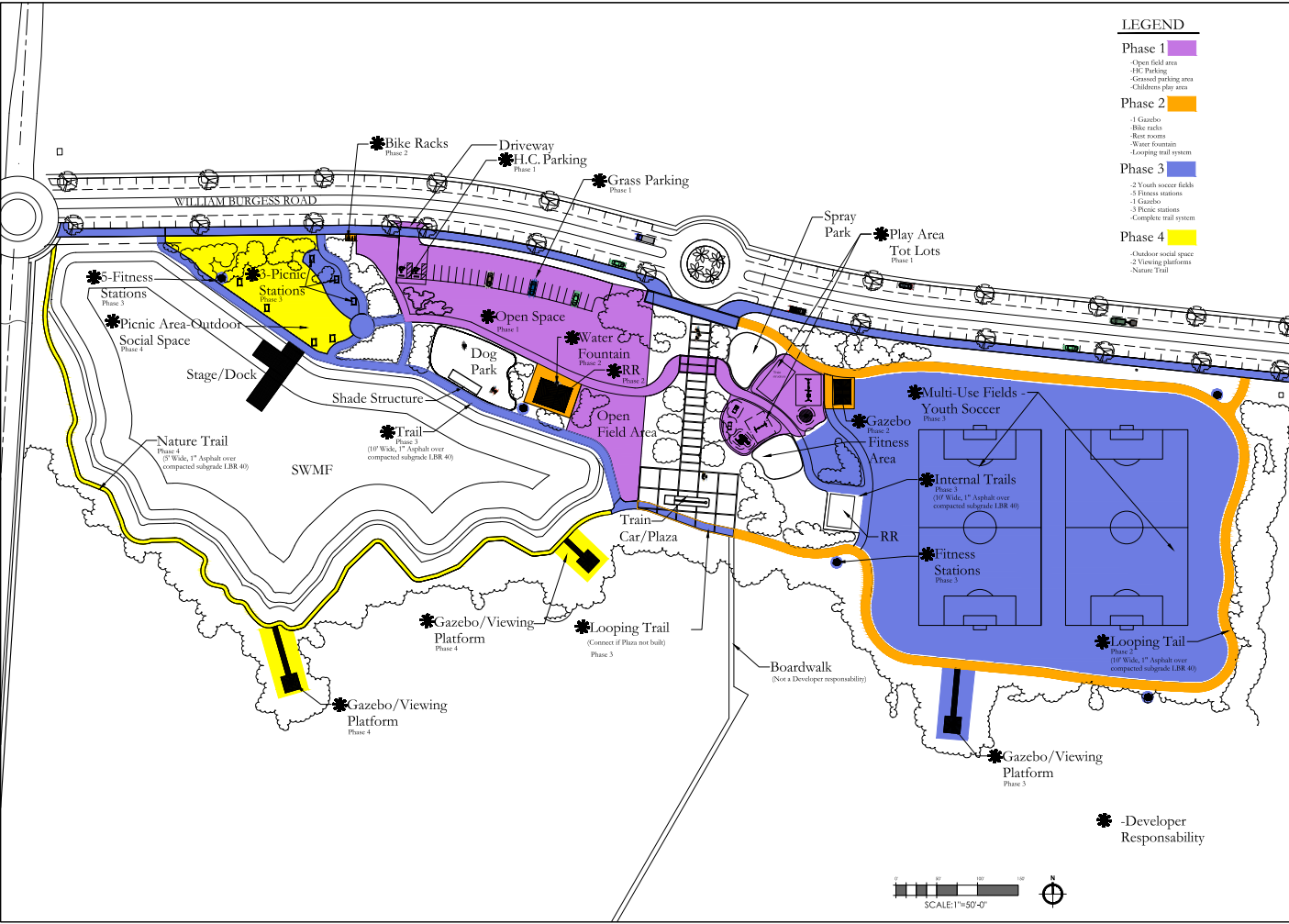


Figure 12. William Burgess Community Park Design

# MULTI-USE TRAIL

## GENERAL

- Where not specified herein, all required bicycle and pedestrian facilities, including Sidewalk Zones as depicted by Street Types defined in Figure 16 on Page 23, shall be constructed contemporaneously with the adjacent site development.
- In lieu of providing sidewalks on both sides of each roadway as required in Section 11.7 of the Roadway and Drainage Standards, the pedestrian and bicycle circulation plan described herein and illustrated in the Preliminary Development Plan shall control. See Figures 17 and 18 on Pages 29 and 30.
- Sidewalks, Sidewalk Zones and bicycle facilities shall be provided to connect the individual components of the development to the multi-use trail system, transit facilities, recreation areas, retail and office uses, and social spaces. See Figure 14 on Page 20.
- The design of the multi-use trail system, sidewalks and bicycle facilities shall be responsive to the placement of utilities and street trees. Utility placement is discussed in the County Fees section of this order, see Page 25.
- Golf carts are permitted to operate on the multi-use trail system within the boundary of the Nassau Crossing development.
- Sidewalks shall be a minimum five (5) feet in width. This is differentiated from the Sidewalk Zone which is of significantly greater width.
- All multi-use trails, boardwalks, sidewalks and bicycle facilities must exhibit a high level of connectivity that best serves the pedestrian, bicyclist and golf cart driver within the Nassau Crossing PUD and providing for connectivity to the greater WBD.
- All segments of the multi-use trail system shall be open to the general public.
- All multi-use trails shall meet the specifications for sub-base, depth and width of asphalt or concrete as defined in the Typical Cross-Section show in Figure 13. The Director of Public Works may approve an alternative sub-base standard if deemed appropriate.
- The Applicant shall be responsible for survey and construction cost of off-site trail segments required by this order, as shown in Figure 14.
- Nassau County will coordinate with other government agencies and utility providers to facilitate the construction of the trail system.
- Sidewalks shall be provided on both sides of all roadways. In cross-sections that include a multi-use trail, a sidewalk shall not be located on either side of the roadway in Parcel A to encourage use of the multi-use trail.

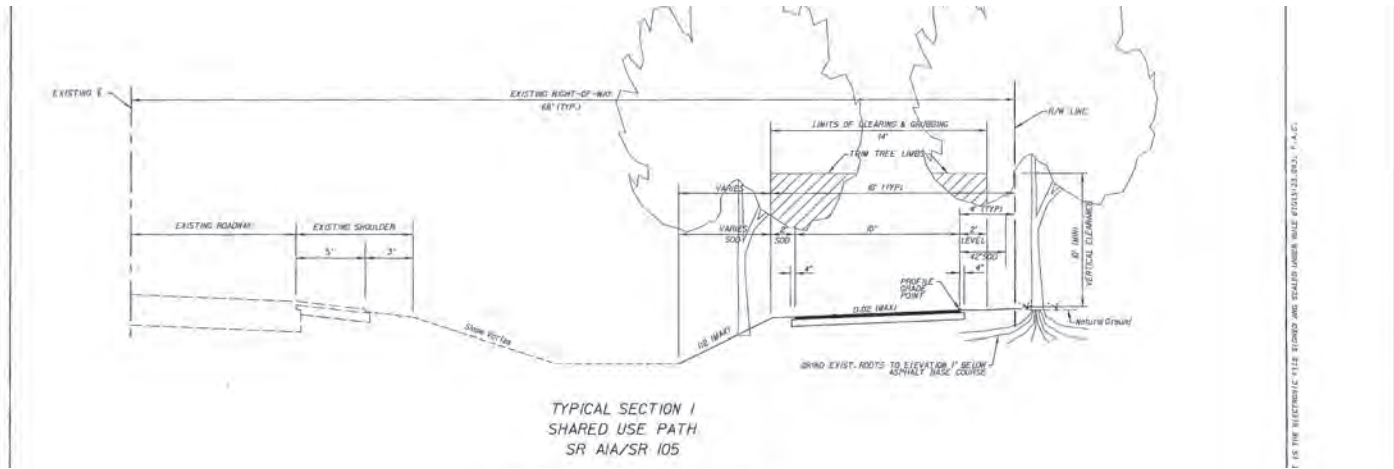


Figure 13. Typical Cross-Sections

## PHASING AND TIMING

- Phase I
  - Phase I is defined as the earlier of the following:
    - The issuance of the 100th Certificate of Occupancy, or,
    - 100th multifamily unit is given a Certificate of Completion, or,
    - 30,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - Prior reaching the above threshold the following shall occur:
    - The Applicant shall construct or cause to be constructed a multi-use trail running on a north-south axis providing for continuous connectivity from the northwesterly most entrance of the development to the intersection with the trail segment running on an east-west axis described in subsection (b) below. See Figure 15. The exact location of the trail through Parcel B may shift but shall be designed in such a manner as to provide easy accessibility to residents and the general public.
    - ~~The Applicant shall construct or cause to be constructed a multi-use trail along an east-west axis parallel to William Burgess Boulevard within Parcel C as generally depicted in Figure 14.~~
- Phase II
  - Phase II is defined as the earlier of the following:
    - The issuance of the 200th Certificate of Occupancy, or,
    - 200th multifamily unit is given a Certificate of Completion, or,
    - 60,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - Prior reaching the above threshold the following shall occur:
    - The Applicant shall construct or cause to be constructed an elevated boardwalk with a minimum width of eight(8) feet between pylons/rails crossing the wetland within Parcel C as depicted in figure 12 on page 18 or, in the alternative, a multi-use trail running parallel and adjacent to Harts Road from the intersection with William Burgess Boulevard accessing the southwesterly most extent of Nassau Crossing PUD unless environmental and/or topographical constraints cause the facility to be narrowed. In no instance shall the pedestrian facility be less than five(5) feet in width. The trail may be constructed on either the east or west side of Harts Road. See Figure 14 on page 20.
    - The Applicant shall construct or cause to be constructed a multi-use trail along an east-west axis parallel to William Burgess Boulevard within Parcel C as generally depicted in Figure 14.
- Phase III
  - Phase III is defined as the earlier of the following:
    - The issuance of the 350th Certificate of Occupancy, or,
    - The issuance of the final Certificate of Occupancy based on recorded plat in Parcel A, or,
    - 250th multifamily unit is given a Certificate of Completion, or,
    - 100,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - Prior reaching the above threshold the following shall occur:
    - The Applicant shall construct or cause to be constructed a multi-use trail from the intersection of William Burgess Boulevard and Harts Road north within the right-of-way of William Burgess Boulevard to the Cook property depicted in Figure 14.
- Phase IV
  - Phase IV is defined as the earlier of the following:
    - The point in time, regardless of the above phasing schedule, William Burgess Boulevard is redeveloped in accordance with the Boulevard Street Type as defined herein and within the WBD, or,
    - 350th multifamily unit is given a Certificate of Completion, or,
    - 150,000 square feet of non-residential space is issued a Certificate of Occupancy or Certificate of Completion.
  - Prior reaching the above threshold the following shall occur:
    - The Applicant shall construct or cause to be constructed a pedestrian crossing of the rail-line at the intersection of the rail road right-of-way and the right-of-way of William Burgess Boulevard. See Figure 14.

Lands within utility corridors can be utilized as amazing trail corridors and connect greenways, public spaces and public parks.

Whitnall Hwy Park  
Burbank, CA



Little Tennessee Trail  
Franklin, NC



Cherry Creek Trail  
Denver, CO



# Multi-Use Trail

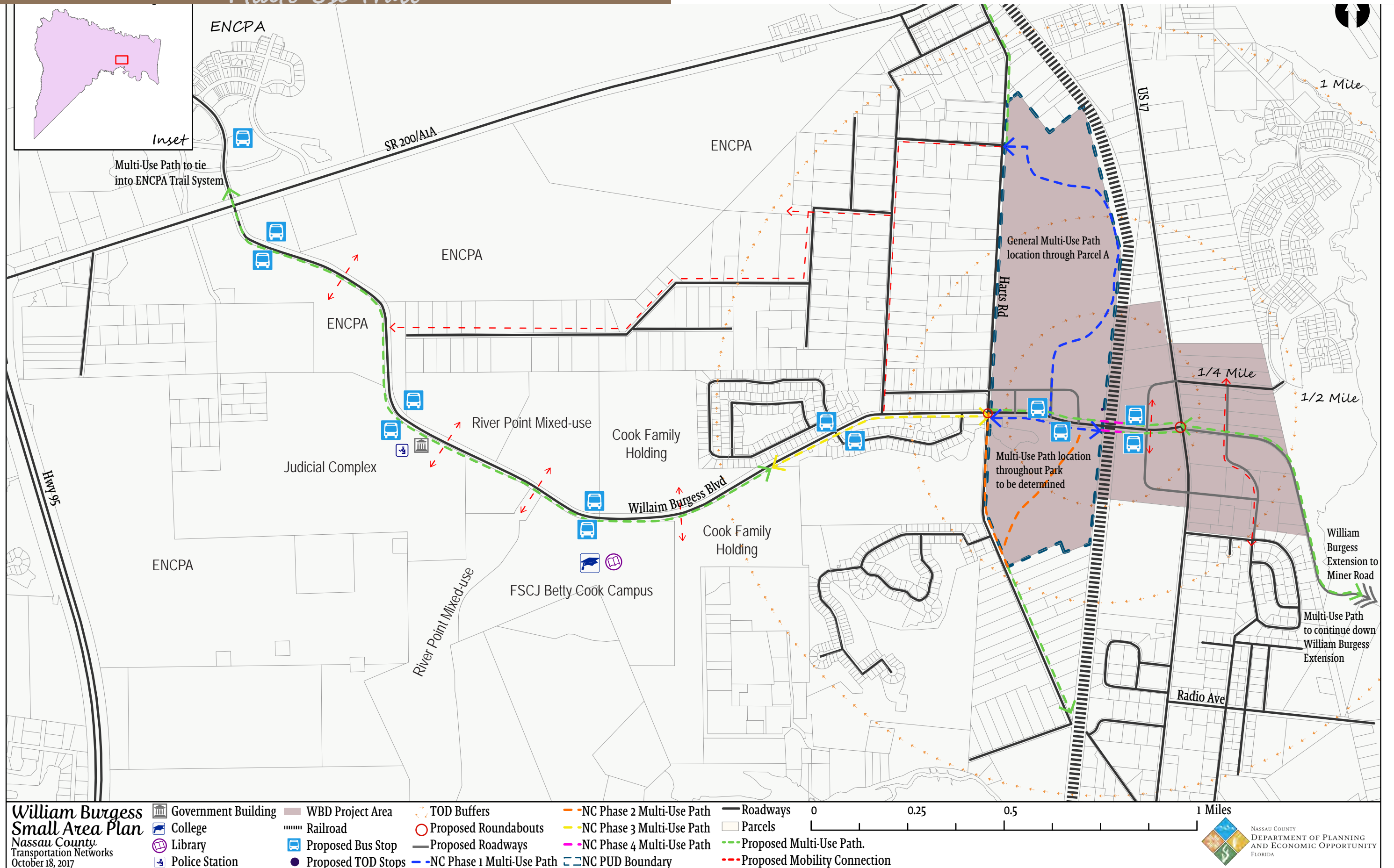


Figure 14. William Burgess Multi-Use Trail

# TRANSIT STATION

## RESERVATION AND DEDICATION:

- The land area depicted in figure 15 totaling approximately five (5) acres shall be reserved for public use to facilitate multi-modal transportation and shared public/social space with the intended development to be for a commuter rail station and shared parking facility.
- Within one-hundred and eighty (180) days of a written request from the County Manager, the land area associated with the commuter rail station and shared parking facility shall be deeded to Nassau County. This dedication shall be by instrument approved by the Board of County Commissioners. The lack of an approved Final Development Plan does not prevent the dedication of land to Nassau County. However, all of Parcel B, including the William Burgess Boulevard re-development plan shall be designed and approved in a single Final Development Plan. The deed of dedication shall reserve an easement and the right to the Applicant or its successors in title to utilize the facilities developed on the parcel and/or to develop such facilities in connection with the development of Parcel B to provide for parking within the William Burgess District.
- The requirement to reserve/dedicate the identified land area will remain in effect for the life of the agreement.
- During the Final Development Plan approval process for Parcel B, the land area reserved to facilitate public transit may be reconfigured but shall not result in a loss of area unless demonstrated to be in the best interest of Nassau County.
- The square footage of the transit station is in addition to the square footages allowed in this PUD.



Figure 15. Commuter Rail Land Dedication

## USES

- It is the intent of the Applicant and Nassau County that the subject land will ultimately serve as a future commuter rail station. The lands around the rail platform, as depicted in Figure 15, will serve as shared public space and integrated with adjacent private development to create a vibrant and socially engaging public realm. The area is intended to be used for entertainment, dining, socializing and general public gathering space.

## INTERIM USES

- The Applicant and Nassau County acknowledge that commuter rail will take some considerable time to develop. As such, the following interim use are permissible.
  - Interim Uses not requiring joint agreement
    - Park and ride facility
    - Bus Rapid Transit facility
    - Bus station/stop
    - Public/Social space for festivals, farmers markets and community events
    - Walking paths, trails and other passive recreation
    - Manicured lawn/field for free play and field sports/youth field sport practice space
    - Frisbee golf course
    - Rope courses and zip-lines
  - Interim Uses requiring joint written agreement from both the Applicant and Nassau County.
    - Any public or non-public use not listed above

It is the Intent of Nassau County and the Applicant that the parking area depicted in Figure 15 is to be used a shared parking facility for all users, visitors and guest of Parcel A, B or C. Nothing herein shall inhibit Nassau County or the Applicant from constructing or causing to be constructed a parking garage in the shared parking location north of the commuter rail station. However, nothing herein shall bind either party to constructing or causing to be constructed the parking garage. If constructed, the structure may house other ancillary retail, service and office uses.



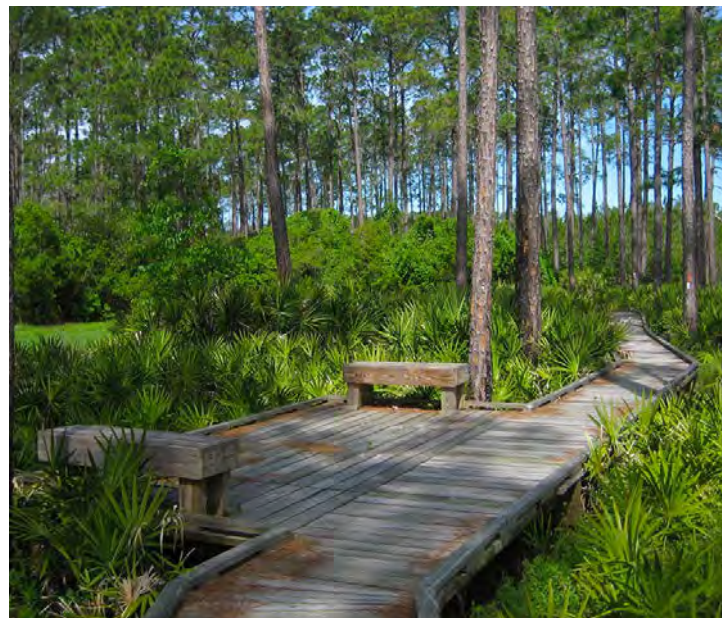
# SOCIAL SPACE/PUBLIC SPACE/OPEN SPACE

## SOCIAL SPACE

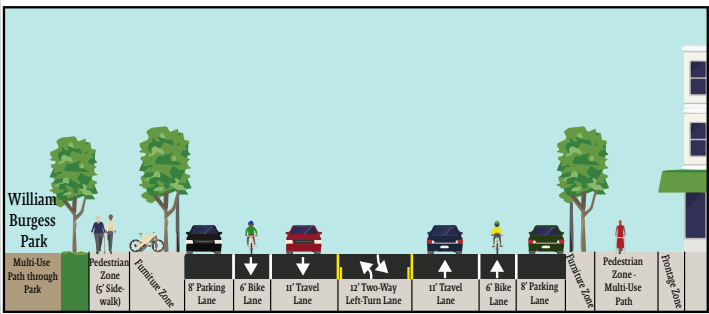
Within Parcel B social space shall be seamlessly integrated within the development program. It is the intent of Nassau County and the Applicant that the entirety of Parcel B promote social interaction through creation of meaningful public spaces. This includes the creation of public space with each development proposal, consistent with the Final Development Plan, that is interconnected and engaged with the surrounding development. Each new development proposal shall provide social space that integrates or will integrate with adjacent development and public facilities. Adjacent projects shall orientate public space so as to create shared spaces in form of manicured lawns, courtyards and similar publicly accessible spaces.

## OPEN SPACES

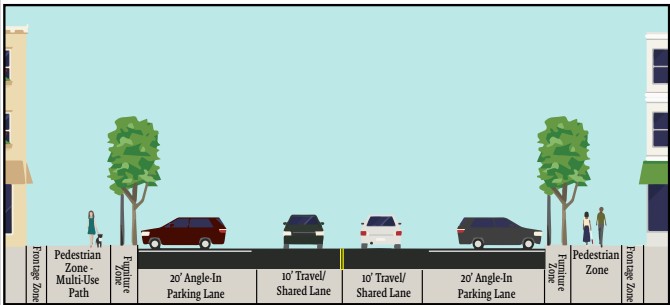
Open space, as differentiated from Social Space and Recreation Space, includes wetland preservation areas, vegetative upland buffers adjacent to wetlands, and wetland mitigation areas may include multi-use trails, nature trails, paths, walkways, boardwalks, viewing platforms, interpretive signage, and other forms of social and recreational amenities shall be permitted subject to applicable state and federal permitting. The exact boundaries of all such areas shall be established on the Final Development Plans for each phase or increment of development.



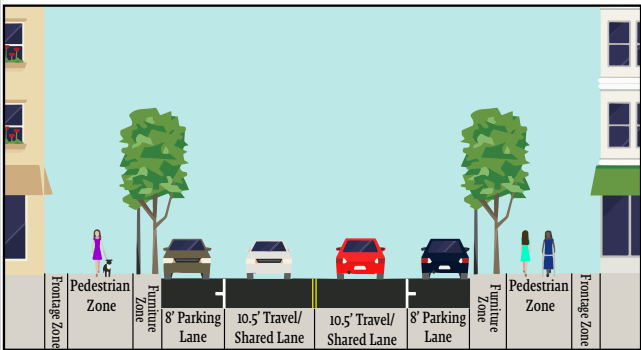
STREET TYPES



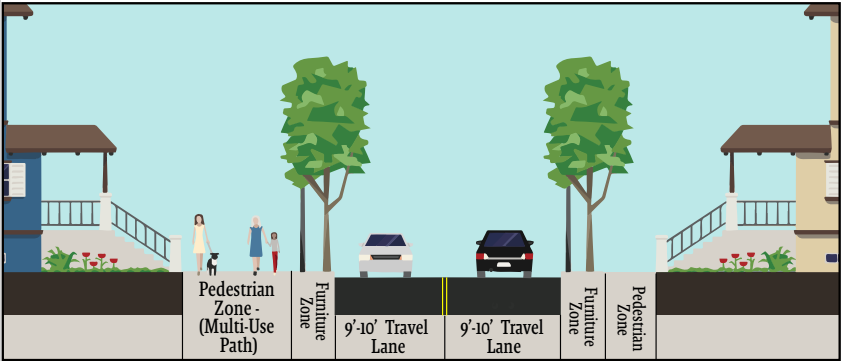
*Street Type: Boulevard*  
The boulevard street type allows for faster movements outside of the primarily commercial area and provides access from SR-200 to US-17. The street width should be no more than 64' curb-to-curb, including two lanes for through/right traffic, a center two-way left-turn lane (TWLTL), two bike lanes, and parallel on-street parking on each side of the road. The total right-of-way should be at least 100 feet. These streets should be designed so that while higher speeds may be facilitated, there are mechanisms to promote pedestrian safety. The TWLTL can be a raised median or pedestrian safety islands where appropriate. Pedestrian activity is expected to be high along this street type due to the park, commercial center, and transit stops (adjacent land uses). Buildings should be brought forward to the frontage zone. The pedestrian zone throughout these streets are either paved sidewalks adjacent to the street and building, or the multi-use path extension. The furniture zone can be used for street trees, lighting, bike parking, benches, trash cans, etc. These street types are found in the village core and center transect zones, however, it should not be used for internal roads.



*Street Type: Main Street*  
A main street is used for the primary road connecting William Burgess Boulevard to internal commercial streets. They too have design treatments focused on pedestrian activity. The street width should be no more than 60' curb-to-curb, including two lanes for diagonal on-street parking. The total right-of-way for this street should be at least 80'. These streets should be designed so that speeds are less than 25mph so that a bicyclist will feel comfortable riding on the street. Pedestrian activity is expected to be high along this street type. Buildings should be brought forward to the frontage zone. The pedestrian zone throughout these streets are either paved sidewalks adjacent to the street and building, or the multi-use path extension. Sidewalks should be a minimum of 6 feet. The furniture zone can be used for street trees, lighting, bike parking, benches, trash cans, etc. These street types are found in the village core and center transect zones.



*Street Type: Commercial Street*  
A commercial street is used for connector streets and are a subset of the main street, as they too have design treatments focused on pedestrian activity. These street types are usually found as "internal commercial streets". The street width should be no more than 48' curb-to-curb, including two lanes for parallel on-street parking. The right-of-way should be at least 70' to include the roadway, furniture zone, and pedestrian zone. However, if bike lanes are required on the roadway as determined by the Public Works Director, the lanes can be widened to accommodate them. Conversely, these streets may be narrowed to have parking on only one side. These streets should be designed so that speeds are less than 25 mph so that a bicyclist will feel comfortable riding on the street. Pedestrian activity is expected to be high along these streets. Buildings should be brought forward to the frontage zone. The pedestrian zone throughout these streets are either paved sidewalks adjacent to the street and building. Sidewalks should be a minimum of 6 feet. The furniture zone can be used for street trees, lighting, bike parking, benches, trash cans, etc. These street types are found in the village core, center, and edge transect zones.



*Street Type: Neighborhood Street*  
A neighborhood street is used for residential purposes. The street width should be no more than 20' curb-to-curb\*. These streets should be designed so that speeds are less than 20 mph. The furniture zone can include street trees, street lighting, and mailboxes. The pedestrian zone can include sidewalks or the multi-use path extension. In some cases, when a pedestrian plan has been approved, sidewalks may be omitted. These streets are generally found the village edge transect zone. \*The applicant may opt to do three (3) nine (9) foot lanes, with one dedicated to marked on-street parking.

William Burgess  
Small Area Plan  
Street Design  
October 19, 2017



BOULEVARD



MAIN STREET



COMMERCIAL STREET



NEIGHBORHOOD STREET

Figure 16. Typical Street Cross-Sections

GENERAL

- a. The street types defined in Figure 16 and the correlating locations within the Nassau Crossing PUD shall be controlling. There shall be no variance to the roadway types.
- b. Utilities and utility easements shall not interfere with the defined street types and required elements/amenities.
- c. The Applicant may construct a roadway connection between Parcel A to Parcel B but such roadway connection shall not be required.
- d. It is the expressed intent of both the Applicant and Nassau County that William Burgess Boulevard not be developed as a 4-lane thoroughfare. As depicted in the Boulevard Street Type, Figure 16, a divided boulevard with pedestrian safe havens and crosswalks are required with an emphasis on a high degree of connectivity, visibility, safety and street/sidewalk pedestrian activation.
- e. It is the intent of the Applicant and Nassau County to activate the street by orientating buildings and amenities to pedestrian at street level.
- f. Bus pull-offs shall be provided within the Nassau Crossing PUD.
- g. Implementation of street improvements consistent with the defined street types shall be contemporaneous with adjacent development in Parcel A.
- h. Parcel A shall be limited to three (3) access points on Harts Road.
- i. Parcel B is limited to three (3) access points on William Burgess Road and one (1) access point on Harts Road.
- j. The internal roadways of the development will be approved by the Nassau County Engineering Services Department and constructed in accordance with the latest Nassau County Construction Details and Specifications, provided however, that cul-de-sacs that have twenty-five (25) or less lots fronting the cul-de-sac shall be allowed to be constructed within a right-of-way having a ninety foot (90') diameter and pavement with a radius of thirty feet (30').
- k. At the time William Burgess Boulevard is reconstructed in accordance with the Boulevard Street Type all overhead utilities shall be place underground contemporaneously with re-development of William Burgess Boulevard or, in the alternative, the overhead power-lines shall be rerouted as to not be seen from the William Burgess Corridor, i.e. rerouted through the existing power-line easement in Parcel A.
- l. The travel lanes within the WBD, including William Burgess Blvd., shall be narrowed to the maximum extent possible. The use of other traffic calming strategies shall be used to the extent practical. The intent of this regulation is to narrow the ROW, slow vehicular traffic and create a tight knit community that is pedestrian friendly. The automobile is a secondary form of mobility.
- m. Operational improvements such as turn lanes, signalization and other similar improvements intend to ensure traffic operations function properly shall be required in accordance with local regulations. With the submittal of each Final Development Plan the operational improvements necessary to support the FDP will be assessed and provided as warranted.



PHASING AND TIMING

- A. Phase I
  - I. Phase I is defined as the earlier of the following:
    - a. The issuance of the 200th Certificate of Occupancy in Parcel A, or,
    - b. Commencement of construction within any portion of Parcel B
  - II. Prior reaching the above threshold the following shall occur:
    - a. The Applicant shall participate in a cost sharing of up to 50% of the cost to construct of a roundabout at the intersection of William Burgess Boulevard and Harts Road. The total cost incurred by the applicant shall not exceeds \$402,000. If ROW is necessary to construct the roundabout the land shall be reserved in Parcel B and Parcel C respectively. The County may choose, at the County's sole discretion, to not participate in the cost sharing of the roundabout. If the County chooses not to participate, the Applicant shall not be required to construct the roundabout until such time as the County is prepared to provide their share of the cost. Nothing herein shall prevent the Applicant or the County from constructing the roundabout earlier than the 200th CO if the County or Applicant desire to provide for the full cost of construction. If the County chooses to not participate in the cost share for the roundabout the applicant/owner shall only be required to construct the minimum improvements (i.e. turn lanes, signalization, etc)as deemed necessary by a traffic study.
- B. Phase II
  - I. Phase II is defined as the earlier of the following:
    - a. The issuance of the 300th Certificate of Occupancy in Parcel A, or,
    - b. The issuance of the final Certificate of Occupancy based on the final phase of the final recorded plat in Parcel A, or,
    - c. Commencement of construction within any portion of Parcel B
  - II. Prior reaching the above threshold the following shall occur:
    - a. No improvement defined with Phase II
- C. Phase III
  - I. Phase III is defined as the earlier of the following:
    - a. Commencement of construction within any portion of Parcel B,
  - II. Prior reaching the above threshold the following shall occur:
    - a. Reconstruct William Burgess Boulevard from Harts Road to US Hwy 17 in accordance with the Boulevard Street Type defined in Figure 16 page 23. This includes placing overhead utilities underground (or relocated), sidewalk zones, on-street parking, streetscaping, street furniture and stub-outs for all cross-streets. If the traffic circle at William Burgess Boulevard and Harts Road is not complete, the construction shall be completed in such a way that the roundabout can be easily added as funds are available.



GENERAL PHASING/FEES/DRAINAGE

PHASING AND COMMENCEMENT OF CONSTRUCTION:

- 1. The development of the residential portion of the project will occur in phases over a period of approximately ten (10) years.
- 2. Construction shall commence on the first phase in Parcel “A” no later than five (5) years from the date of approval of the PUD. Parcel “A” may be broken down into multiple phases and permitting and physical development within each phase may occur as market conditions dictate.
- 3. Development of the Parcel B will occur as market conditions dictate with no established time frame for commencement or completion. However, the development of Parcel B shall be subject to a single Final Development Plan that includes all of Parcel B, the re-development of William Burgess Boulevard and a master stormwater management plan that takes into consideration all related improvements. While individual components of Parcel B may be constructed as market conditions dictate, each individual development shall be soundly demonstrated to fit within the context of the Final Development Plan, Nassau Crossing PUD and WBMOD. No development shall commence within Parcel B until such time a master drainage plan has been completed and approved by Nassau County for Parcel B.

UTILITIES:

- A. Underground water and sewer service will be provided by JEA.
- B. Underground electric service will be provided by Florida Power & Light.
- C. Fire hydrants will be located on final development plans and will be constructed in accordance with applicable County and JEA requirements.
- D. Nassau County will support the applicant/developer in working with the utility providers to strategically place utilities as to not interfere with the defined street types and required street trees, street furniture and amenities.
- E. All utilities shall be under ground.

SITE DRAINAGE

- 1. It is the intent of Nassau County and the Applicant that the entirety of the Nassau Crossing PUD and the requisite improvements to William Burgess Boulevard from US 17 to Harts Road be subject to a master stormwater management plan that limits, to the maximum extent possible, stormwater management facilities being located within the Core 1 Transect(Parcel B) see the Preliminary Development Plan on Pages 29 and 30.
- 2. No development shall commence within Parcel B until such time a master drainage plan has been completed and approved by Nassau County for the build-out of Parcel B. The Final Development Plan for Parcel B shall show all stormwater facilities necessary to facilitate full buildout.
- 3. All stormwater facilities that are deemed by Nassau County to be unavoidable within the Core I Transect (Parcel B and C) shall be integrated with the design to function as an aesthetic feature and amenity. The provision of fencing around ponds shall be prohibited. The banks of all ponds shall be sloped in such a manner as to eliminate the need for fencing. This includes expansion of the existing stormwater management facility located on Nassau County property in the southeast quadrant of the intersection of Harts Road and William Burgess Boulevard.
- 4. The development of the Property shall meet all applicable requirements of the St. Johns River Water Management District and Nassau County.
- 5. The retention areas and other elements of the drainage system outside of the road rights-of-way serving the single family development will be maintained by a Property Owners’ Association and will be subject to appropriate easements prohibiting the construction of accessory uses that would interfere with any drainage or maintenance.
- 6. The retention areas and other elements of the drainage system for Parcel B will be owned and maintained privately by the owners of such parcels. The retention areas and other elements require to provide site drainage for Parcel C shall be owned and maintained by Nassau County.
- 7. The Applicant shall be responsible for including Parcel C and the future transit facility in the master drainage plan.
- 8. All St. Johns River Water Management District and Nassau County permits shall be obtained prior to commencement of construction, pursuant to Ordinance 2000-40, as amended. The developer shall obtain an operating permit for these facilities before transferring them to any homeowner’s association or organization.

IMPACT FEES AND MOBILITY FEES

Based on the current fee schedule the development will generate the following estimated impact fees for use by the County for the provision of Public Facilities based on the development program defined in the PUD.

- A. Impact Fee Credits shall be governed by Sec. 7.06 of the Comprehensive Impact Fee Ordinance in effect at the time of approval of this PUD and shall be subject to a Credit Agreement approved by the County Manager and Board of County Commissioners. The transferability of credits to other owners and properties within the respective impact fee district located outside of the Nassau Crossing PUD shall be considered in said credit agreement. Nothing herein shall prevent credits being transfered to other owners and properties within the boundary of the Nassau Crossing PUD.
  - I. It is the intent of Nassau County that recreation impact fee credits will be given for the construction of all public recreation facilities provided for by the PUD and for the dedication of land for public recreation facilities, including linear facilities, ~~the off-site multi-use trail/sidewalk required to be constructed along Harts Road south of William Burgess Boulevard and along William Burgess Boulevard from Harts Road to the Cook Property~~, and the construction of the boardwalk and internal system of multi-use trails within Parcel C. Fee amount is subject to change with updates to the County’s impact fees.
  - II. Actual impact fees will be calculated based on the fee amount at the time of building permit unless otherwise provided for in the above referenced Credit Agreement. The table below is an estimate.
- B. Mobility Fee Credits shall be governed by Sec. 3.02 of the Nassau County Mobility Fee Ordinance in effect at the time of approval of this PUD and shall be subject to a Credit Agreement approved by the County Manager and Board of County Commissioners. The transferability of credits to other owners and properties within the respective Mobility Fee Zone located outside of the Nassau Crossing PUD shall be considered in said credit agreement. Nothing herein shall prevent credits being transfered to other owners and properties within the boundary of the Nassau Crossing PUD.
  - I. It is the intent of Nassau County that mobility fee credits will be given for the re-construction of William Burgess Boulevard including the traffic circle at William Burgess Boulevard and Harts Road as well as the dedication of land for the shared parking facility, commuter rail station, traffic circle right-of-way, the off-site multi-use trail/sidewalk required to be constructed along Harts Road south of William Burgess Boulevard and along William Burgess Boulevard from Harts Road to the Cook Property, and any other land dedicated for mobility improvements.
  - II. Actual Mobility Fee will be calculated based on the fee amount at the time of building permit unless otherwise provided for in a separate agreement. The table below is an estimate. Fee amount is subject to change with updates to the Mobility Fee Plan.
  - III. Receipt of mobility fee credits are subject to an update of the Nassau County Mobility Plan/Schedule of Capital Improvement.

Type of Fee	Single Family (350du)	Multi-Family (450du)	Retail* (150,000 sf)	Office ** (150,000 sf)
Mobility	\$402,500	\$363,150	\$306,720	\$222,900
Administration	\$298,200	\$345,150	\$154,200	\$82,650
Fire/EMS	\$56,350	\$65,250	\$29,250	\$15,600
Police	\$25,900	\$30,150	\$13,500	\$7,200
Recreation Comm. Park	\$100,450	\$129,600	0	0
Recreation Reg. Park	\$112,000	\$116,550	0	0
School	<del>\$1,143,800</del> 1,900,710	<del>\$1,470,600</del> 2,443,770	0	0
TOTAL	<del>\$2,139,200</del> 2,896,110	<del>\$2,520,450</del> 3,493,620	\$503,670	\$328,350

\* Calculation of Mobility Fee was based on the following:  
30,000sf of Restaurant, ITE Code 932 (\$2,170 per 1,000sf = \$65,100)  
30,000sf of Shopping Center, ITE Code 820, 1sf - 9,999sf (\$2,150 per 1,000sf = 64,500)  
90,000sf of Shopping Center, ITE Code 820, 10,000sf – 99,999sf (\$1,968 per 1,000sf = \$177,120)

\*\* Calculation of Mobility Fee was based on the following:  
30,000sf of Medical Office, ITE Code 720 (\$2,541 per 1,000sf = \$76,230)  
30,000sf of General Office 1sf – 9,999sf, ITE Code 710 (\$1,009 per 1,000sf = \$30,270)  
30,000sf of General Office 10,000sf – 49,999sf, ITE Code 710 (\$1,434 per 1,000sf = \$43,020)  
60,000sf of General Office 50,000sf – 99,999sf, ITE Code 710 (\$1,223 per 1,000sf = \$73,380)

# SIGNAGE

1. All signage, regardless of parcel or phase, shall be controlled by a unified signage program. Nassau County has contracted with Vanasse Hangen Brustlin, Inc to create a unified lighting and signage program for the WBD, adopted with the first Final Development associated with the Nassau Crossing PUD. The signage program shall be consistent with the aesthetic/theme of the WBD and Nassau Crossing PUD. No permanent signage or entry feature may be constructed until the unified signage program is adopted.
2. Entry features may be constructed at each project entrance in Parcel A. The entry features shall be consistent with the aesthetic/theme of the WBD and Nassau Crossing PUD. The entry features are intended to promote a sense of place and define ones location. Entry features may include sculpted earth into the design.
3. The entry features may include project identification signs. The signs at each entry to Parcel A may be single-faced, double-faced or may include two (2) separate signs, one on each side of the entrance. All project signs for Parcel “A” shall be designed as ground-mounted signs or integrated into or mounted on the landscape or architectural features such as a wall or public art piece.
4. While elements of the entry feature may exceed these dimensions, the signable area shall not exceed one-hundred and fifty (150) square feet. In no way shall any entry feature inhibit access by the general public to the multi-use trail, sidewalks or bicycle facilities.
5. Entry features, public art and aesthetic terrain design shall be provided at the intersection of William Burgess and Harts Road and the intersection of William Burgess and the rail-line right-of-way. These entry features shall possess a strong relationship to the aesthetic/theme of the WBD and Nassau Crossing PUD. These features will define the node and are integral to placemaking. These entry features shall be included in the unified signage program.
6. Signage within Parcel B shall be incorporated into the design of the urban environment and not adversely impact the aesthetics of the streetscape. It is the expressed intent of the Applicant and Nassau County that a suburban scale strip commercial signage program is prohibited. See page 37 and below for examples of permitted signage. Also see the William Burgess District Vision Book.
7. Billboards shall be prohibited.



Do This



Do This



Do This



Not This



Not This



Not This



Do This



Do This



Do This



## PERMITTED SIGNAGE EXAMPLES



\*Replace stackstone with tabby stone or Florida keystone



Do This



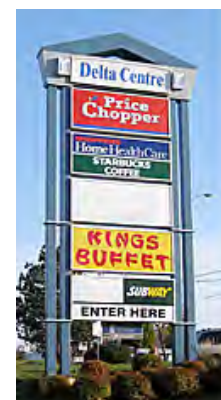
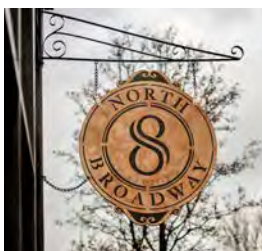
Not This



Not This



Not This



Not This



Do This



Not This



Never This

# LANDSCAPING

- (a) All sites shall meet the minimum standards for landscaping defined in Sec. 37.05 of the Nassau County Land Development Code unless expressly stated otherwise in the Nassau Crossing PUD or WBD Vision Book. The Planning and Zoning Board may waive or modify aspects of the landscape requirements when demonstrated to be directly implementing the supplemental design guidelines and intent of the WBD Vision Book pursuant to subsection 15 on page 15 of the Nassau Crossing PUD. The Planning and Zoning Board will consider the intent of the WBD, the relationship of the building/landscape material to the street, and impact to other properties. Financial implications on the applicant/site developer shall not be considered.
- (b) A streetscape plan shall be submitted with each application for development. The streetscape plan shall depict the sidewalk zones and correlating features such as, but not limited to, street furniture, street trees, pedestrian areas and on-street parking. Native canopy trees shall be provided along all streets spaced no more than thirty (30) feet on-center. See the typical cross sections and sidewalk zones provided for on page 23 Figure 16.
- (c) Native canopy trees shall be planted along pedestrian and bicycle facilities at a spacing of no more than thirty (30) feet on-center to provide shading and define the non-automobile spaces.
- (d) Where possible and appropriate, landscaping should be native and wild in expression allowing the natural/native under-story to flourish. Incorporating swaths of natural vegetation into a project design is encouraged.
- (e) All landscaping shall have permanent irrigation.
- (f) Building walls that are 25' in length with no windows or a pedestrian pass-through shall utilize landscaping, in addition to architectural elements described in the Nassau Crossing PUD and WBD Vision Book, to soften the facade.
- (g) Landscaping shall be placed on the outside of any wall to provide visual interest.
- (h) The use of planters, tree wells and other similar elements of urban design shall be incorporated into the landscape program.
- (i) All public/social spaces, including but not limited to, greens, courtyards, squares, etc, shall incorporate landscape programs, in addition to architectural element such as pergolas, into the design in such a manner that the space is aesthetically inviting and provides shade to encourage use.

